

Dave Dicke Real Estate Administrator D-2 ODOT Bowling Green

***Innovative design ideas that save right of way acquisition costs
Service roads, retaining walls, modified design, bridge span
extension***



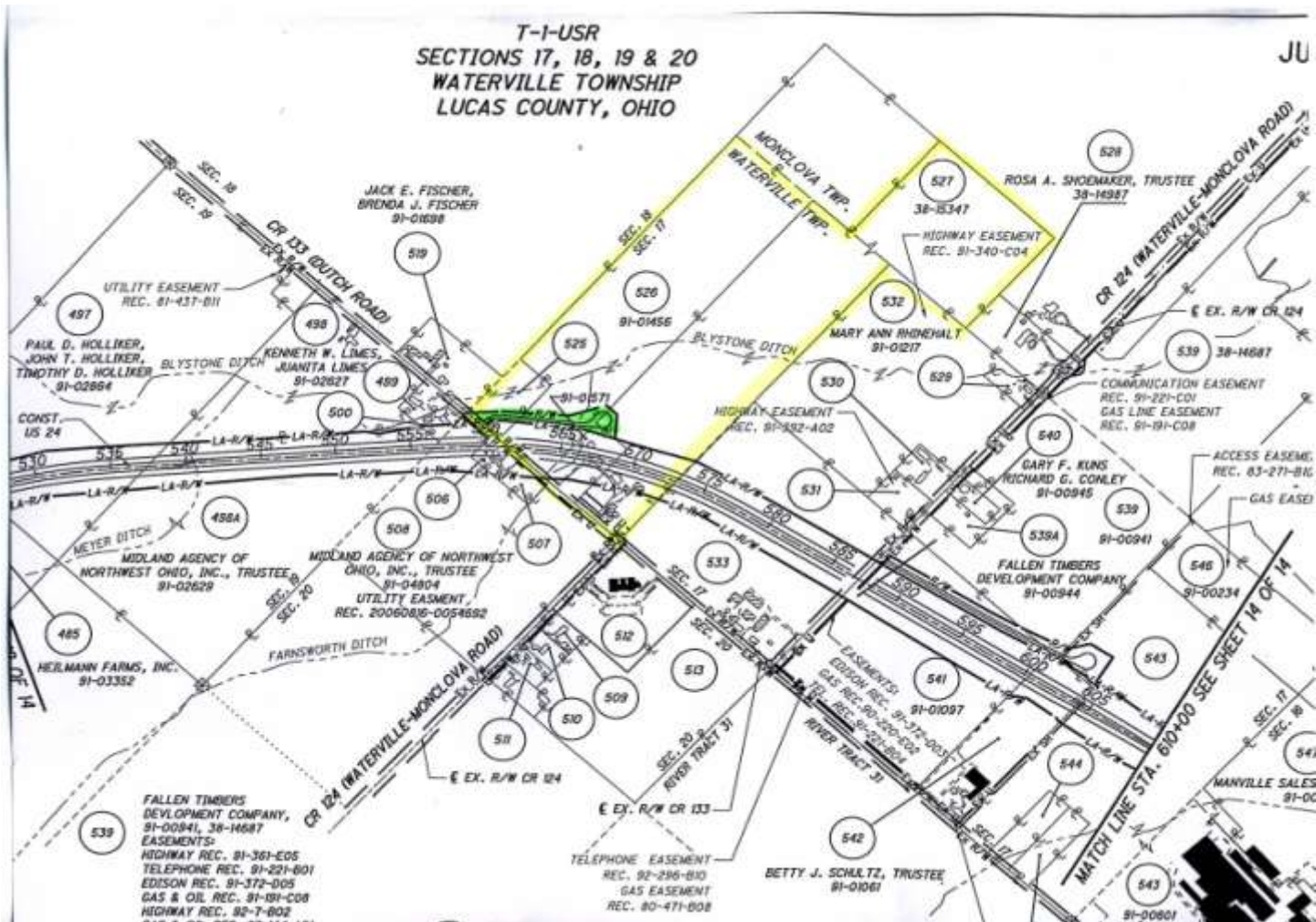
My discussion will identify 3 projects that had various plan modifications that reduced r/w costs

- 1 HEN/LUC-24 a new 4 lane free way Major New Project
service road additions to access remainders of high value
partial retaining wall to reduce slope saving a rail siding yard
- 2 LUC- Wheeling Road widening adding lanes LPA project
lane width reduction, curb width reduction, minor change to road grade reduced
right of way impacts significantly
- 3 SEN Jones Road Rail Grade Separation
bridge extension maintained access to 3 industrial plants in order to greatly
reduce right of way costs, relocation of enterprises and potentially save jobs

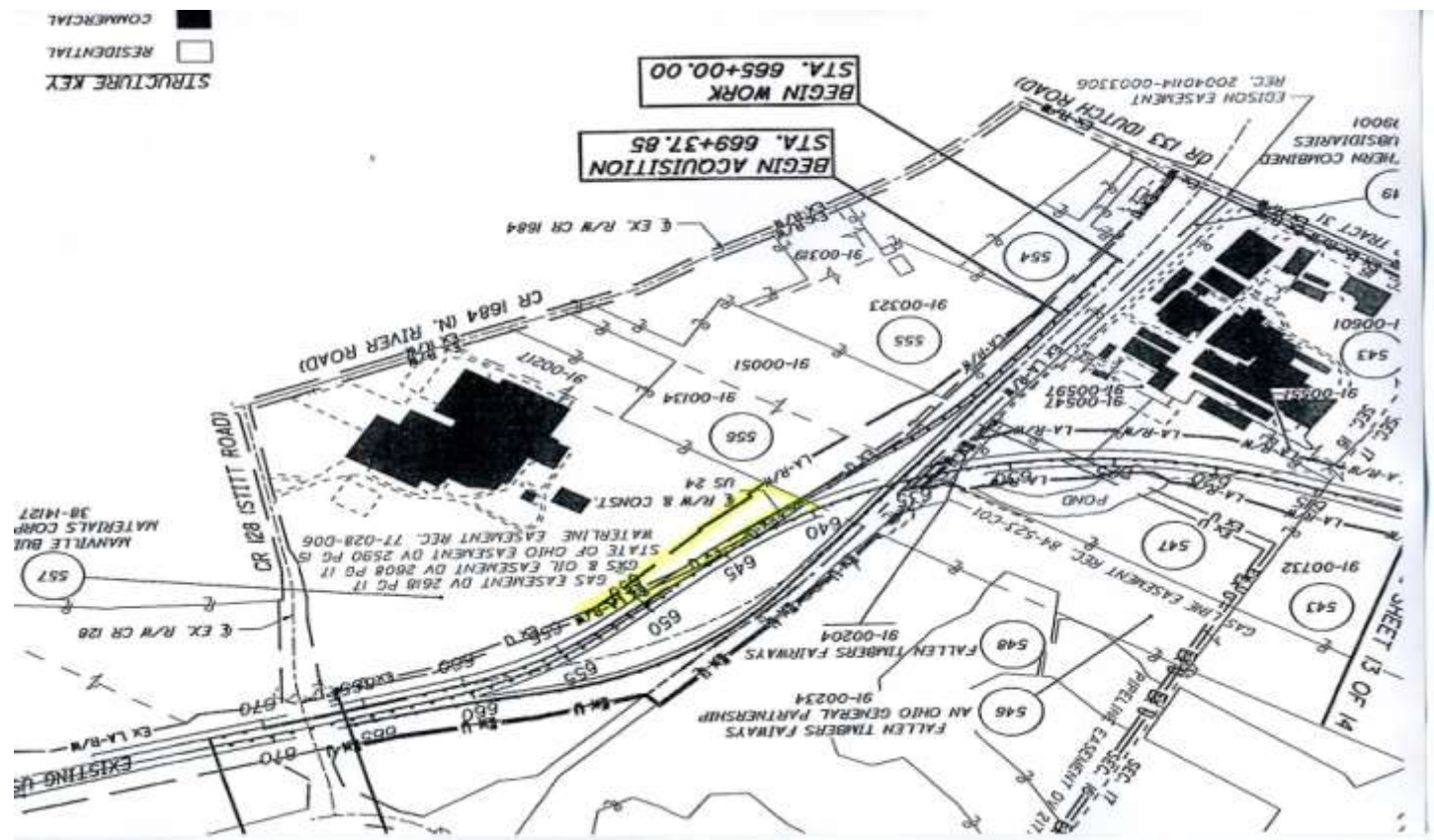
HEN/LUC-24 service road addition saves +/- \$ 1,300,000.



Stage one plans did not include the green shaded service road. Without it +/- 73 acres at a before value of +/- \$30,000 per acre would be legally land locked at a damage of +/- \$ 1,500,000. parcels are owned by family but under different title. The service road costs +/- \$200,000. including design, thus saved +/- \$ 1,300,000.



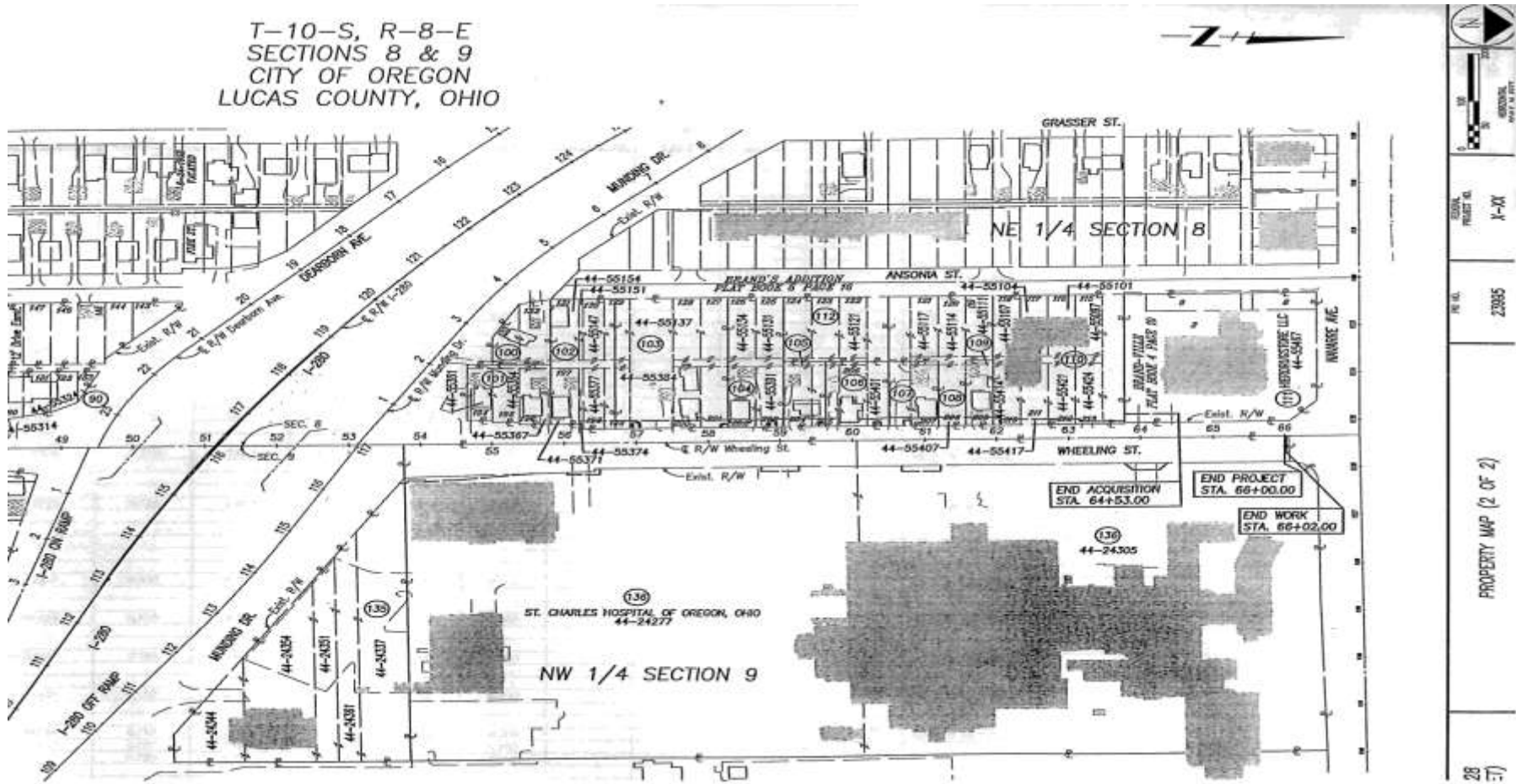
HEN/LUC-24 stage one plans included an earth slope embankment that would remove most of a 4 rail siding for a major manufacturer. Wetland and other site restrictions would prohibit reconstruction/relocation of the required siding. The plant relies on rail shipping for its products. Financial loss or costs were in estimated at in excess of \$ 3,000,000.



The earth slope was reduced by designing a partial retaining wall structure estimated at a cost of +/- \$ 600,000. including design. Only about 400 Lineal feet of rail siding was impacted. Extensions of the unaffected rails at a cost of +/- \$ 200,000 restored capacity. Thus the savings was in excess of +/- \$2,000,000.



Project 2 LUC- Wheeling Street an LPA minor arterial widening.
 Stage one plans included 5- 12 foot wide lanes, +/- 6 foot wide paved berms and 2 foot curbs. The effect would take 5 or 6 homes and significant parking.



After design discussion and study 2 lanes were reduced to 10 foot wide berms and curbs reduced to provide a safe design and similar level of service. The result no homes taken due to widening and no parking lost. This reduced r/w costs in excess of +/- \$1,000,000.



Project 3- Known as Jones Road a Rail Grade Separation in Seneca County in the City of Fostoria

The extension of the original bridge design structure saved significant right of way funds

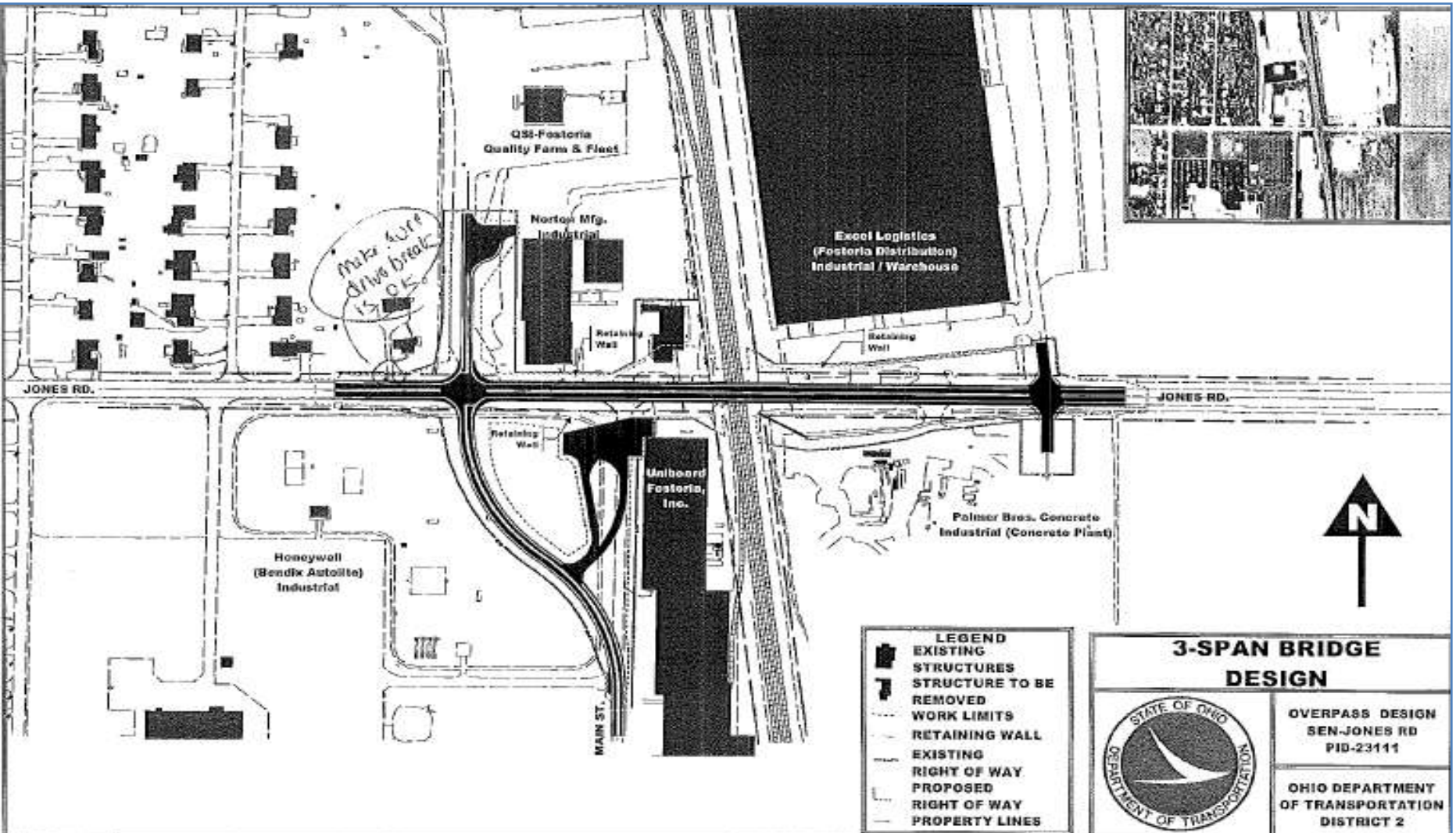




2008 8 27

Jones road is a busy local arterial with light industrial and warehousing near the at grade rail crossing. The project was funded from the State Rail Grade Separation program.

Conceptual plans were prepared, public meetings held and the project began the design process. As the plan progressed and right of way estimates were prepared it became obvious that 3 enterprises would be significantly effected.



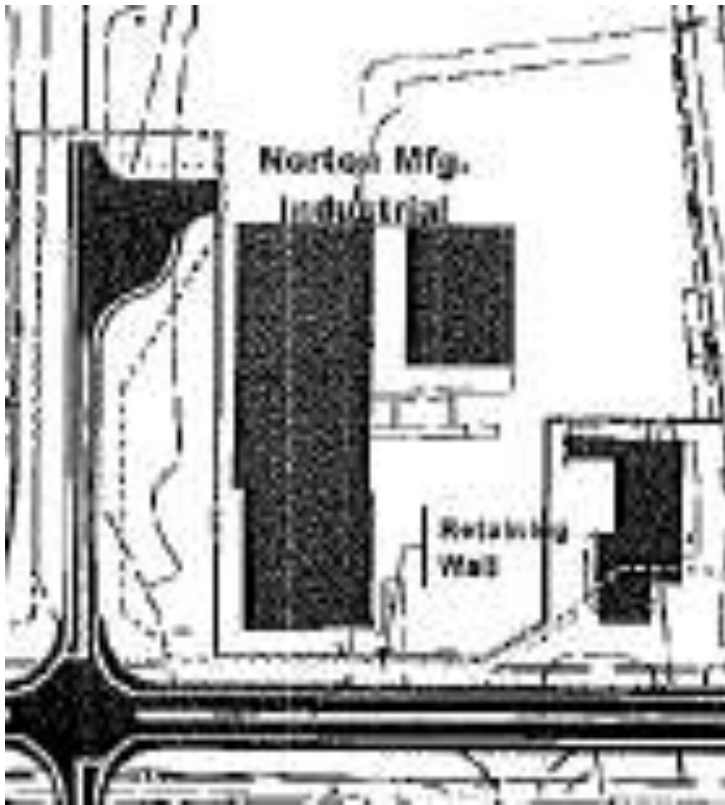
- 1 AF Leibengood's building business would be removed est. cost \$200,000. plus
- 2 Norton Mfg would loose its direct access and internal access would such the relocation may be required est. cost \$1,200,000 plus
- 3 Uniboard Fostoria Inc. would loose access to its saw dust recovery system, loading docks thus significant internal relocation and costs would be required to maintain operations est. costs \$600,000 plus.





- 1 AF Leibengood's building business would be removed est. cost \$200,000. plus

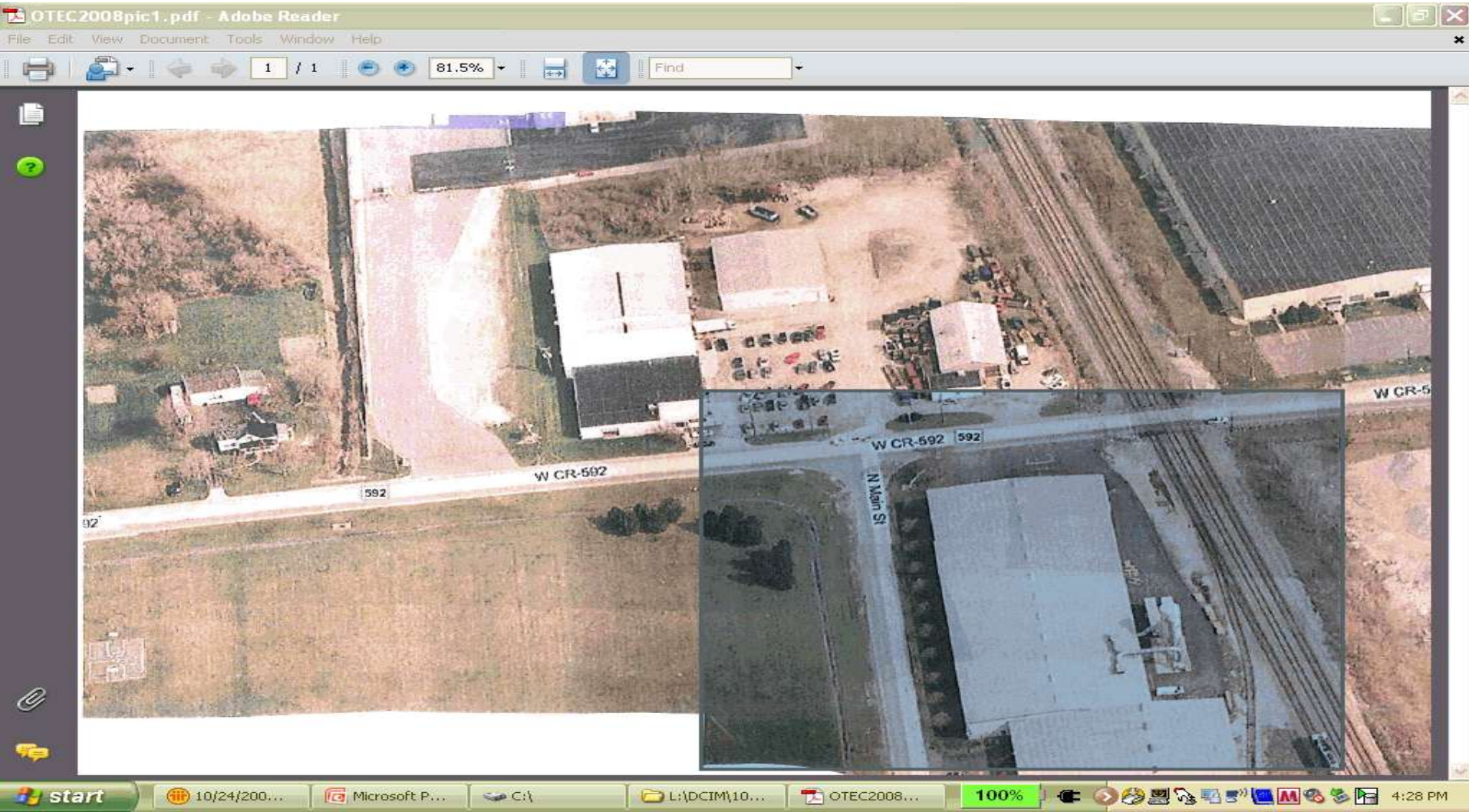
- 2 Norton Mfg would lose its direct access and internal access such that relocation would likely be required est. cost \$ 1,200,000 plus

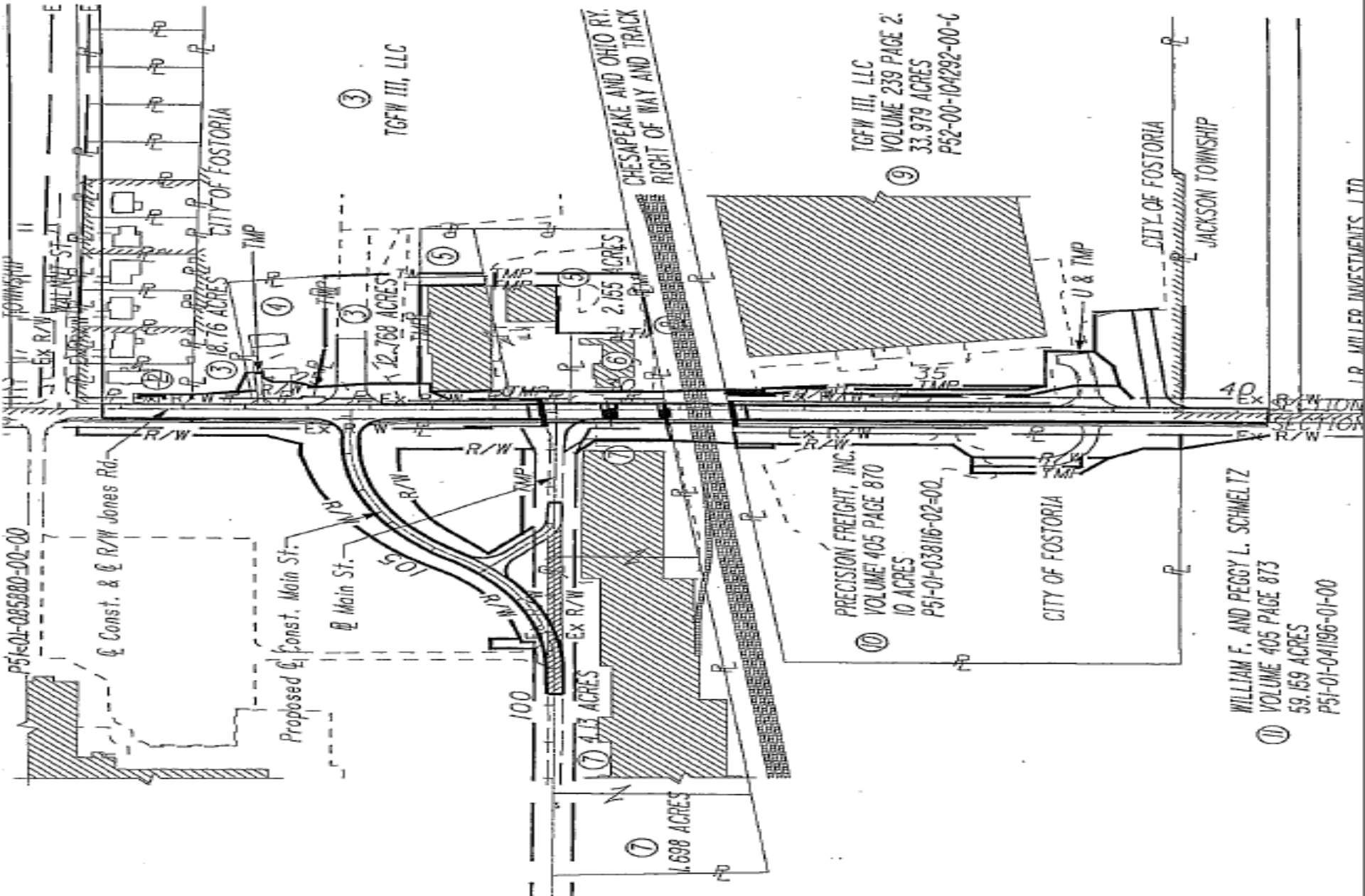




2008 8 27

- 3 Uniboard Fostoria Inc. would lose access to its saw dust recovery system, loading docks thus significant internal relocation and costs would be required to maintain operations est. costs \$ 600,000 plus.





P51-01-035880-02-00

Const. & Ex R/W Jones Rd.

Proposed Const. Main St.

Ex Main St.

4.3 ACRES

7

1.608 ACRES

18.76 ACRES

3

TGFW III, LLC

12.768 ACRES

3

2.155 ACRES

6

PRECISION FREIGHT, INC.

10 ACRES

P51-01-038116-02-00.

10

TGFW III, LLC

VOLUME 239 PAGE 2.

33.979 ACRES

P52-00-104292-00-C

9

CITY OF FOSTORIA

CITY OF FOSTORIA

JACKSON TOWNSHIP

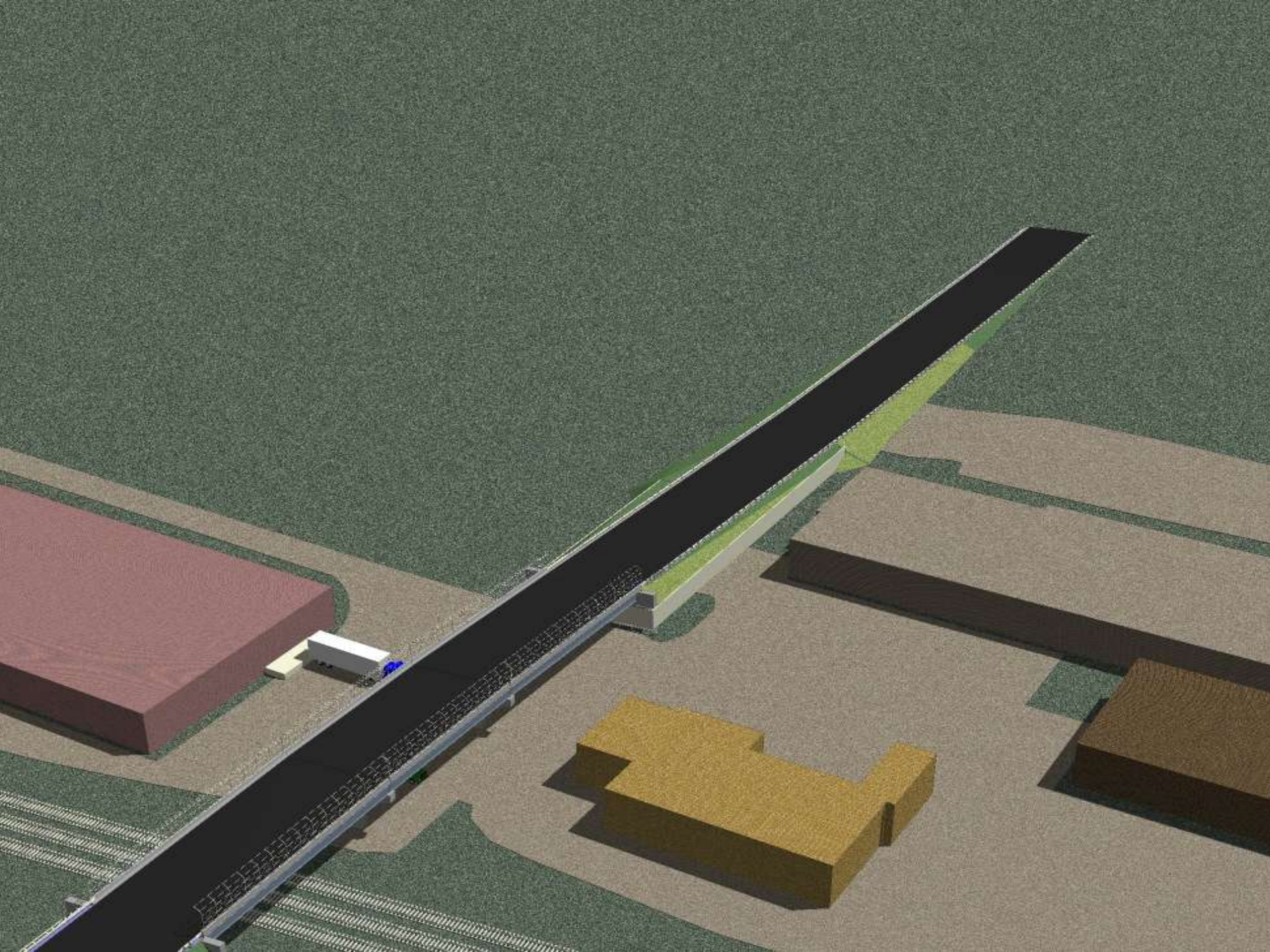
WILLIAM F. AND PEGGY L. SCHMELTZ

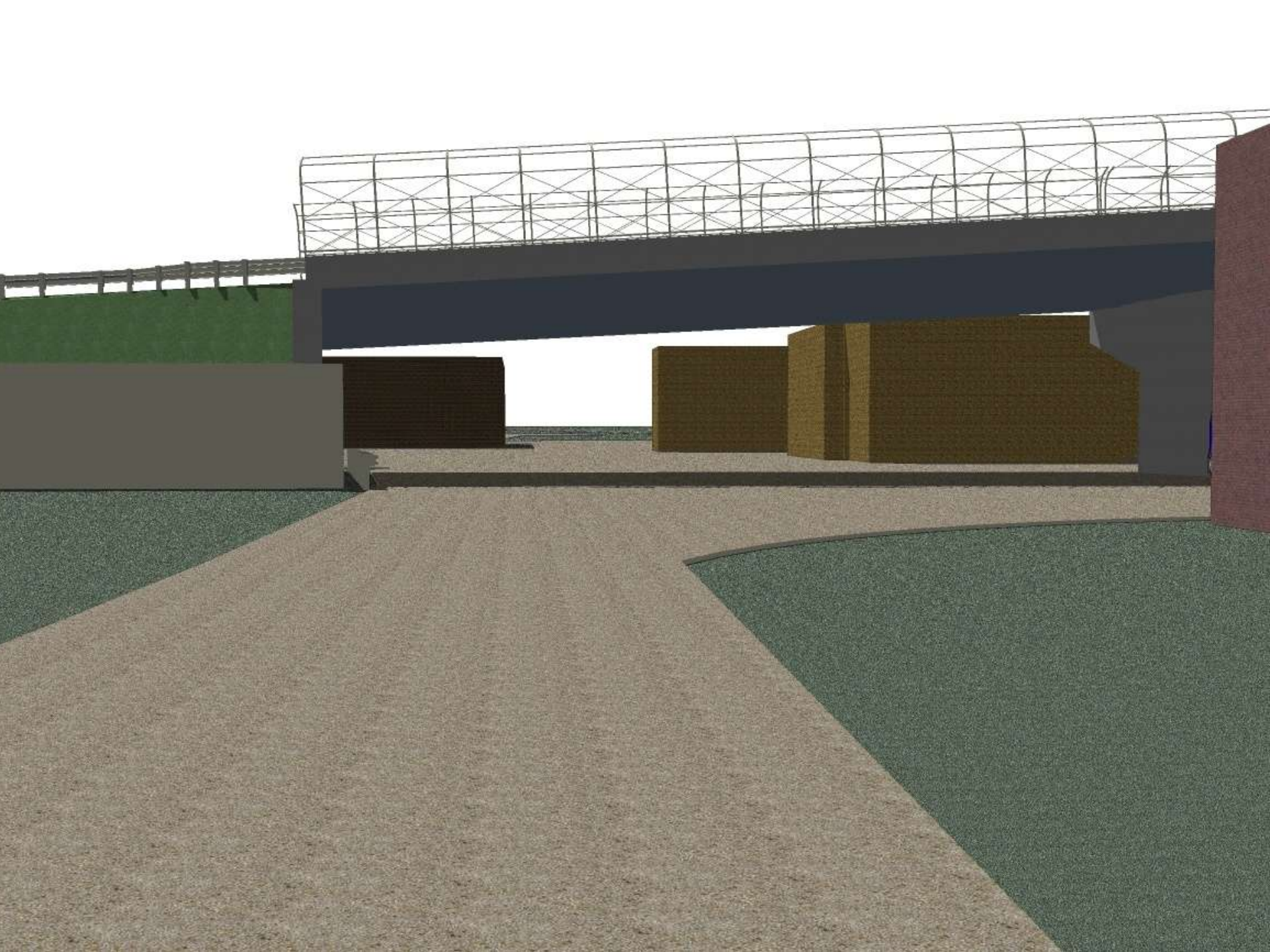
59.159 ACRES

P51-01-041196-01-00

11

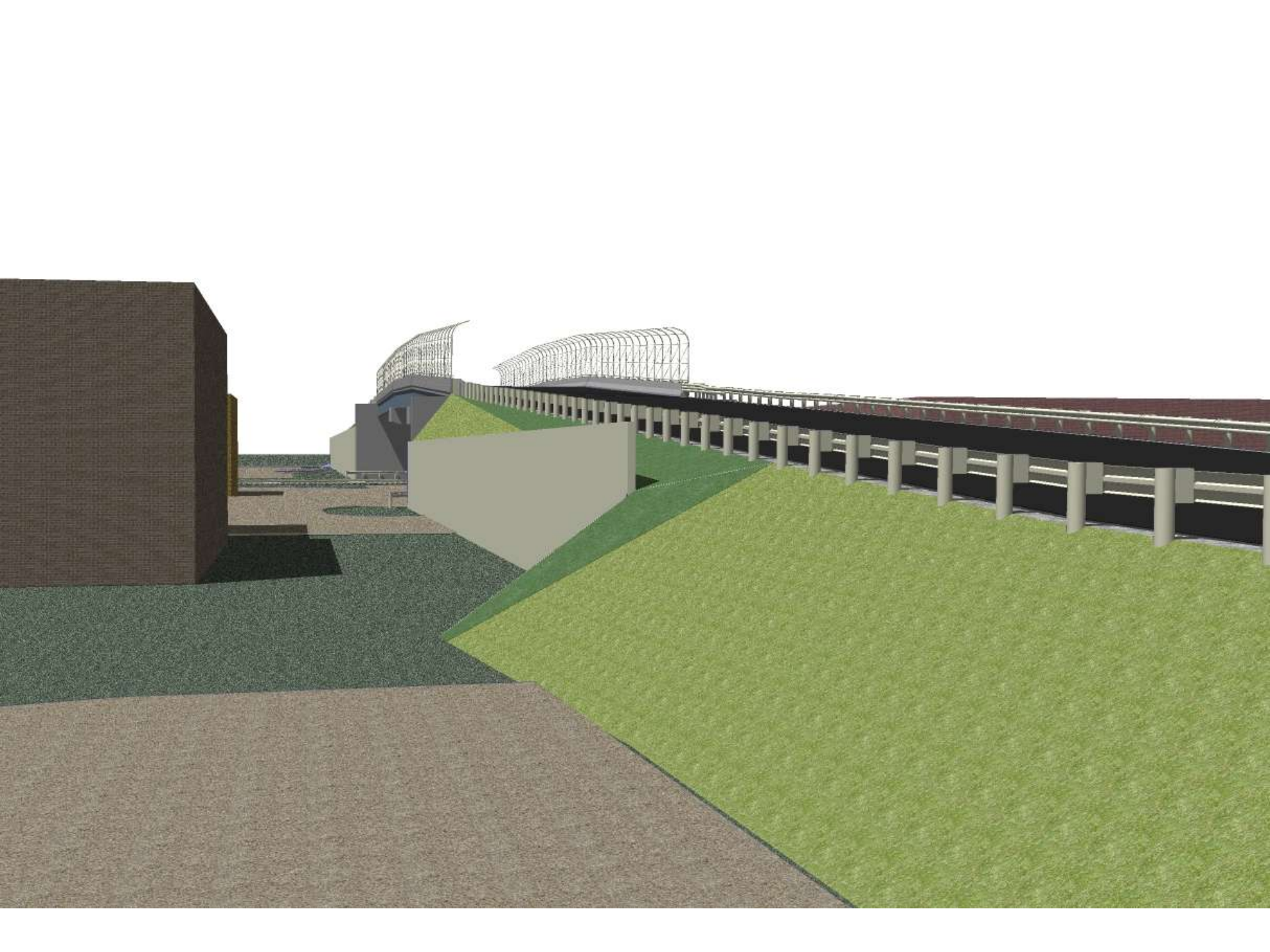
I.R. MILLER INVESTMENTS, LTD





CALLIES
PERFORMANCE PRODUCTS INC.

2008 8 27





***Design modifications on these three projects saved in excess of
\$ 6,800,000. +/-***

***Designers and Planners need to continue to focus more on creative
alternatives to reduce right of way impacts and costs***

