



OHIO RAIL DEVELOPMENT COMMISSION



Ohio's Intermodal Railroad Terminals

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Overview of Ohio's Intermodal Railroad Terminals

Ohio has long been the “Heart of it All” when it comes to business and commerce. Ohio’s industries can access 60% of the US and Canadian population as well as 62% of US manufacturing and retail stores within 600 miles of the state’s borders.

When it comes to “intermodal transportation,” that is by moving containers and trailers on trains and truck, Ohio is truly a premier intermodal freight hub. With 13 facilities distributed across the state, Ohio is second only to Illinois in the number of intermodal terminals. Intermodal transportation is one of the many reasons Ohio is a leader in industry and commerce, providing a strategic location from which businesses produce, assemble and distribute their finished goods. Combined with our other world class rail, highway, water and air facilities, Ohio’s intermodal network is ready to serve whatever business needs arise.

This guide to Ohio’s Intermodal Railroad Terminals serves as a resource for existing and potential new shippers, logistics and economic development professionals, public officials and other stakeholders in Ohio’s freight transportation and intermodal industry. Last year, 11.8 million containers and trailers moved in intermodal service in the US. This guide identifies the intermodal transportation services available at strategic locations around the state, in and near Ohio’s major metropolitan areas. Ohio’s Class 1 railroads, Norfolk Southern (NS) and CSX Transportation, serve these intermodal terminals. This guide provides maps of the CSX and NS intermodal rail networks and explains how these railroads are improving their services to better serve Ohio’s transportation needs.



Heart of it all...
13 facilities
within a **600**
mile radius of
62% of US
manufacturing
and retail

Glossary of terms

1 Intermodal Terminal

While the term “intermodal” refers to many different interactions between transportation modes, for this guide the term “intermodal terminal” refers only to facilities where mechanized loaders and cranes transfer containers and trailers between trucks and rail cars. Intermodal terminals enable businesses to capitalize on the economies and fuel cost savings of rail while still utilizing the short-haul or “last-mile” benefits of truck pick-up and delivery.



Overhead cranes

2 Doublestack Intermodal Trains

In 2011, US railroads moved 10.1 million containers and 1.7 million trailers via intermodal terminals. The reason for the wide difference between intermodal container and trailer traffic is that containers can be double stacked while trailers cannot, which in most cases makes the economics of moving containers on trains superior to moving trailers. Doublestack trains require higher clearance above the tracks of up to 20’2”. While there are already many routes in Ohio that are cleared for doublestack train service, both CSX and NS have been actively expanding their networks’ ability to move doublestack container trains. In addition to the work being performed by the railroads themselves, Ohio has participated in Public Private Partnerships (PPP) to expand rail network capabilities to move double stack

trains with both CSX and NS because of the logistics benefits of intermodal to Ohio shippers. The National Gateway Project is a PPP with CSX and seven other states that increases clearances on its lines from its new Northwest Ohio Terminal in North Baltimore to multiple ports along the East Coast by raising bridges, lowering tracks, and other means. The Heartland Corridor PPP, a collaboration with NS, the federal government and the states of Ohio, West Virginia and Virginia, that established the new Rickenbacker Intermodal Yard in Columbus and raised clearances to enable double stack trains between the ports of Virginia and Columbus. In 2010, the State of Ohio, the Ohio Kentucky Indiana Council of Governments and NS partnered to extend doublestack clearance from the Rickenbacker Intermodal Yard in Columbus to the two existing NS intermodal yards in Cincinnati.



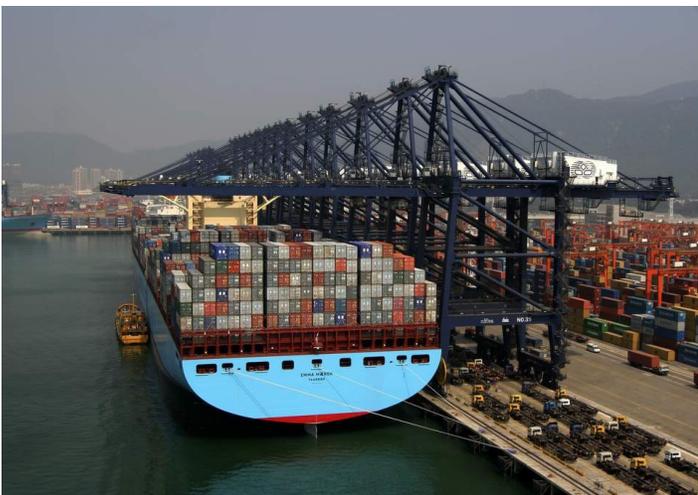
Doublestack freight train

3 International Containers and Domestic Containers

As the name implies, international containers travel between the US and other countries, generally by ship. International containers are measured in twenty foot equivalent units (TEUs) and can be 20, 40 or 45 feet in length; they are 8 feet wide, and are generally 8'6" tall or less. Shorter containers are typically used for heavier loads such as machinery and supplies of steel, while lighter consumer goods are transported in longer containers.

International containers are built to be stacked on ships 5 or 6 high.

In contrast to international containers, domestic containers are meant to replace truck trailers. Domestic containers can be of a variety of sizes for specific uses, but are generally 53' long, 8'6" wide, and 110" high, just like truck trailers. Domestic containers are only built to be stacked 2 high on trains.



4 TOFC/COFC, Well and Spine Cars

The acronyms “TOFC”, or Trailer on Flat Car, and “COFC”, or Container on Flat Car, are still in wide use in the intermodal industry. However, the overwhelmingly predominant rail car used to carry containers is no longer the flat car. The well car has all but replaced the flat car in intermodal container service. As the name implies, containers sit low in a well of the

rail car to enable lower clearance requirements. While some well cars can also be configured to handle trailers, most trailers are handled on spine cars, which are specifically designed to accommodate trailers. Spine cars can carry trailers or containers. However, because spine cars are not configured with “wells,” containers on spine cars cannot be double stacked due to clearance restrictions.

A typical well car

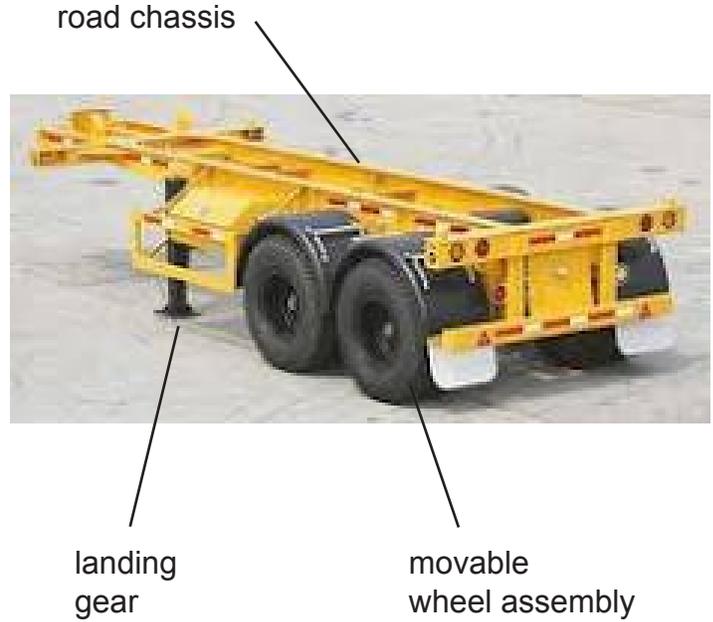


A typical spine car



5 Truck Chassis

The truck chassis is a single-purpose semitrailer designed to haul shipping containers over the road. Containers are either loaded onto the truck chassis by a crane or off-loaded to a rail car.



6 RoadRailer®

RoadRailer® is a service offered exclusively by Triple Crown Services Company, a Norfolk Southern subsidiary. RoadRailer® are specially designed hybrid trailers that function both as a road trailer and a rail car. Mounted on rail wheels (“bogies”), RoadRailer® technology eliminates the

need for well or flat cars, cranes, and other specialized equipment. This service is marketed to provide lower costs, faster train assembly, and more secure transit. Triple Crown provides transportation service throughout the eastern two-thirds of the U.S. and to Toronto and Montréal, Canada, from its Sandusky terminal.



7 Balance

The efficient utilization of the intermodal fleet is one of the more challenging aspects in the management of intermodal freight traffic. Balance refers to maintaining equilibrium between inbound loads and outbound loads at any given terminal so that the movement of empty equipment is minimized. For example, an intermodal terminal that receives many loaded inbound international containers, but does not have export loads for outbound movements, would likely have to reposition

containers, i.e. send them back to steamship companies empty, which is a cost disadvantage. In addition, there could be a shortage of chasses at this unbalanced terminal because many more trucks would be leaving with chasses than trucks entering with them. The imbalance of an intermodal terminal, however, can present opportunities. Freight handlers and logistics companies establish strategies to minimize these operating costs. This can result in competitive pricing to achieve balance and eliminate empty equipment movements.

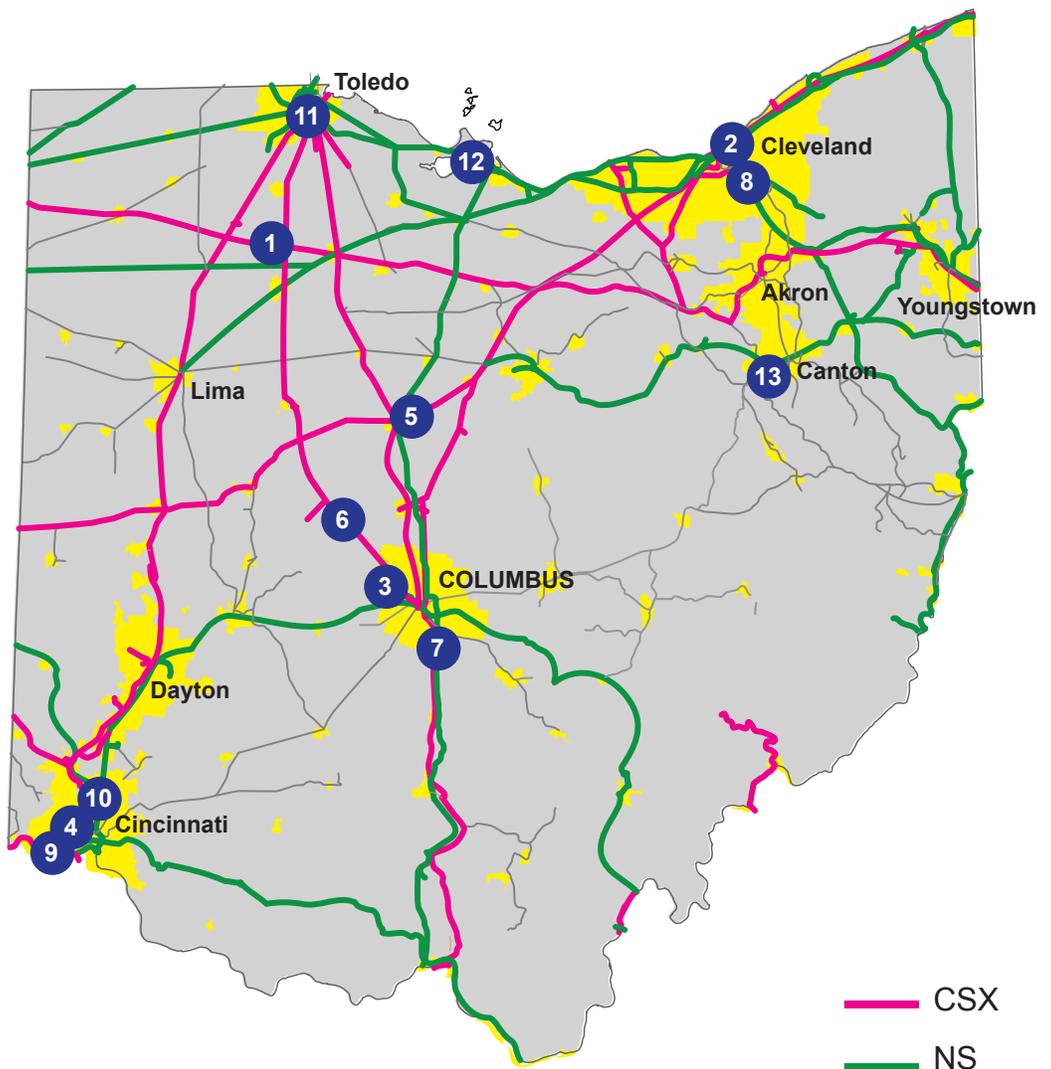


Intermodal Railroad Terminal Locations in Ohio

Ohio has 13 intermodal terminal facilities located throughout the state. CSX and NS each serve 6 terminals. While the Stark Neomodal facility (operated by WL&E) is designed and equipped as an intermodal yard, it does not currently receive intermodal containers. The intermodal terminal locations are identified on the table and map below.

Ohio is second only to Illinois in the number of intermodal terminals

- 1 NW Ohio ICTF (CSX)
North Baltimore
- 2 Collinwood (CSX)
Cleveland
- 3 Buckeye Yard (CSX)
Columbus
- 4 Queensgate (CSX)
Cincinnati
- 5 Marion Intermodal Center (CSX)
Marion
- 6 Marysville (CSX)
Marysville
- 7 Rickenbacker (NS)
Columbus
- 8 Maple Heights (NS)
Cleveland
- 9 Gest Street (NS)
Cincinnati
- 10 Sharonville Intermodal Terminal (NS)
Cincinnati
- 11 Toledo Airline Junction Intermodal Terminal (NS)
Toledo
- 12 Triple Crown (NS Subsidiary)
Sandusky
- 13 Stark Neomodal (WLE)
Navarre



Accessing Intermodal Services in Ohio

There are many ways for companies to access intermodal services. These services can be contracted directly with the serving railroad or steamship lines, or through third party logistics providers (3PLs). While there are many 3PLs operating in Ohio, the contacts for the Class 1 railroads serving the state are listed below. They can serve as an entry point for any company considering the use of intermodal services. What follows is a brief description of the Class 1 intermodal services in Ohio as well as detailed location maps.

CSX Intermodal

Kim Sherman
614.848.3651
Kim_Sherman@csx.com

For more information visit:
<http://bit.ly/KHx5oZ>



NS Intermodal

Randy Bayles
757.664.5173
Randy.Bayles@nscorp.com

For more information visit:
<http://bit.ly/LiFQKJ>



CSX Intermodal Services in Ohio

(Description provided by CSX Transportation)

CSX Corporation, based in Jacksonville, Florida, is one of North America's seven Class I railroads and its extensive network of intermodal and rail-to-truck transload services operates throughout the eastern half of the US. The company's transportation network spans 21,000 miles, with service to 23 eastern states and the District of Columbia. CSX's network connects more than 240 short line and regional railroads and more than 70 ocean, river, and lake ports. Ohio boasts more track miles on CSX than any other state on the network, and includes six intermodal terminal locations in Cincinnati, Cleveland, Columbus, Marion, Marysville, and North Baltimore.

As part of the National Gateway Corridor program, the railroad is increasing its clearances from the East Coast to Ohio, home to the most technologically advanced, environmentally friendly intermodal terminal in North America. The CSX Northwest Ohio intermodal terminal opened in 2011 and is often referred to as the intermodal terminal of the future. Operating as a network hub, the terminal handles over 30 trains per day with service to over 25 different markets by completely redesigning the intermodal service CSX provides to its customers.

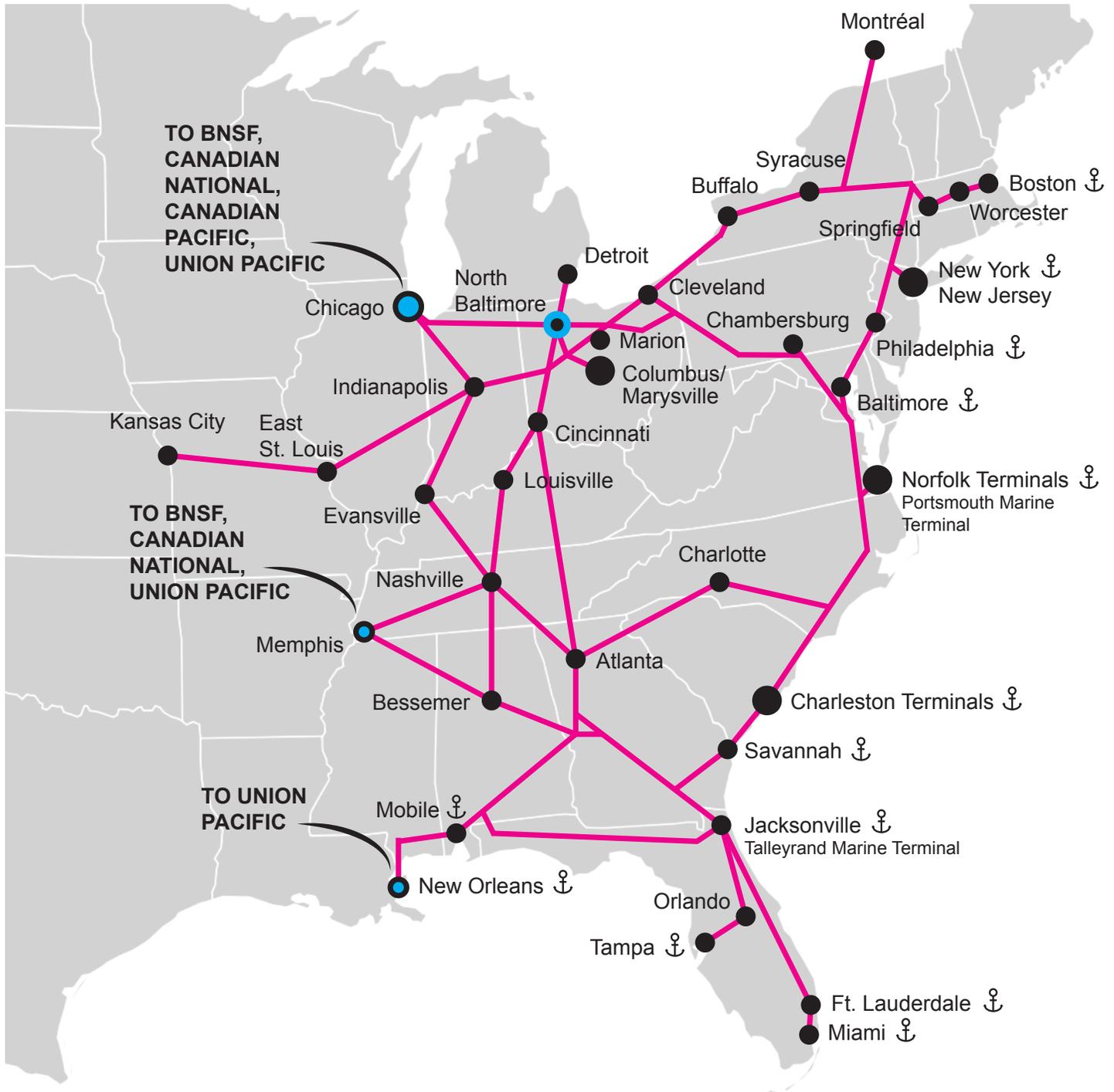
The North Baltimore Terminal sports the latest in technologic advances, while also adhering to CSX's stringent environmental standards. For example, CSX brought into the terminal five wide span electric cranes to support yard operations, the first of their kind in North America. The cranes create no emissions in their operations and in fact generate a portion of the electricity required to operate them. Furthermore, this new terminal will serve as a catalyst for future development and economic opportunities throughout the region. CSX is also engaged in terminal expansions and upgrades in Columbus. Buckeye Yard on Columbus's west side is already under

construction, with a completion date of 2013. The new terminal layout in Columbus will introduce wide span electric cranes, similar to the technology employed in North Baltimore. The end result at this terminal will double its capacity and improve operating efficiency and service to CSX customers in Central Ohio.

Currently, CSX's Cincinnati, Columbus, Cleveland, and Marysville Intermodal Terminals have direct connections with the new Northwest Ohio Terminal at North Baltimore. With these direct connections, these terminals are linked to any port or terminal on the CSX Intermodal network, and to the ports and intermodal terminals of CSX's partner railroads.

Ohio boasts more track miles on CSX than any other state on the network, and includes intermodal terminal locations in Cincinnati, Cleveland, Columbus, Marion, Marysville, and North Baltimore

CSX System Intermodal Network Map



RAIL MAP SYMBOLS

CSX TRANSPORTATION, INC.

● Terminal

● Multiple Terminals

● NW Ohio Intermodal Transfer Facility (N. Baltimore)

● Interchange Facility

⚓ Marine Terminal

CSX NW Ohio Intermodal Container Transfer Facility

Terminal Contact Information

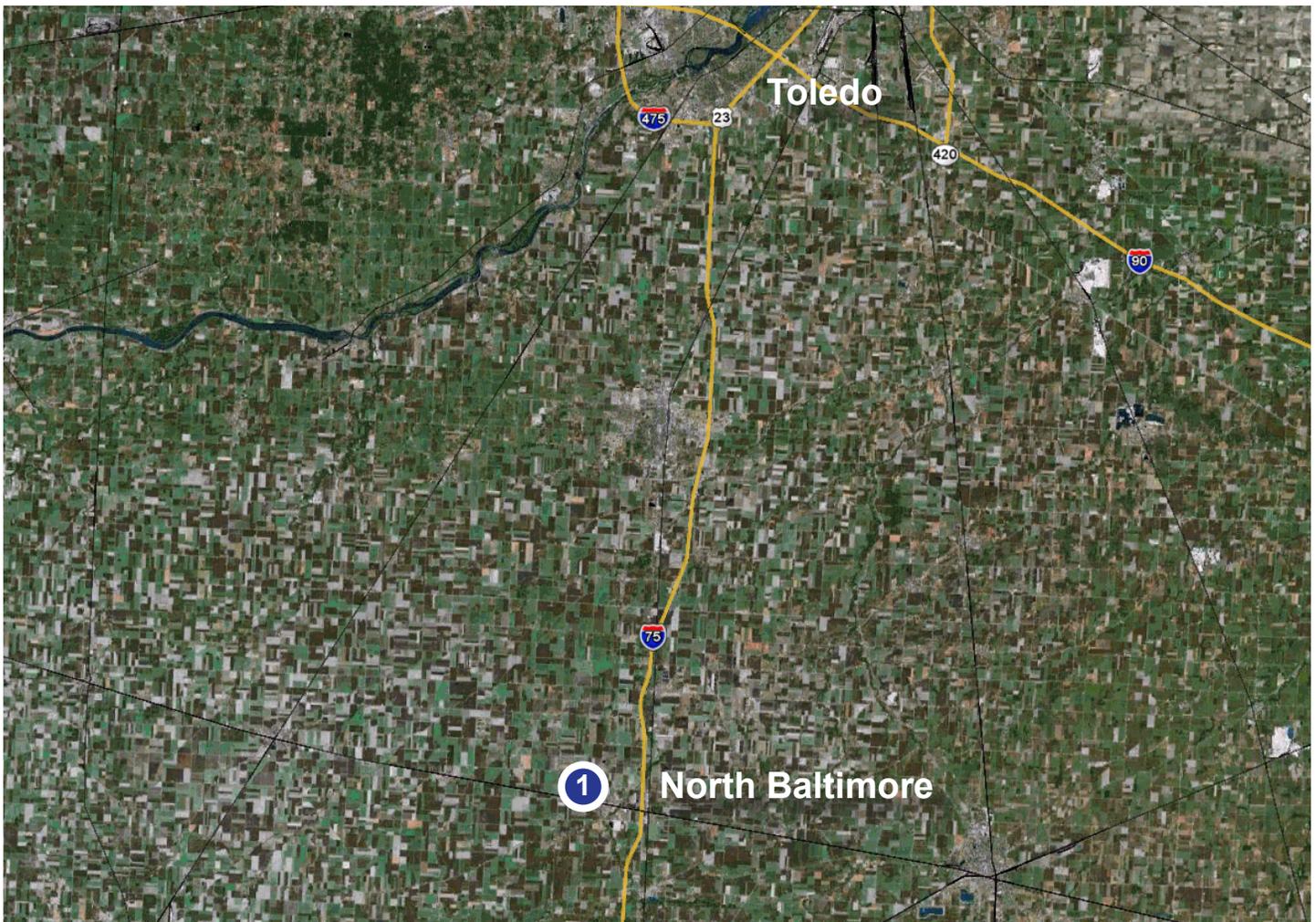
Location 17000 Deshler Road
North Baltimore, OH 45872

Phone 419.257.1231

Terminal Operations

Hours of Operation 7 days a week
24 hours a day

Terminal Capabilities COFC + TOFC



Directions

From I-75, take Exit 167, State Route 18, and turn west. Follow SR 18 for about 3 miles. The main entrance to the North Baltimore terminal is on the right.

CSX Collinwood Intermodal Terminal

CLEVELAND

Terminal Contact Information

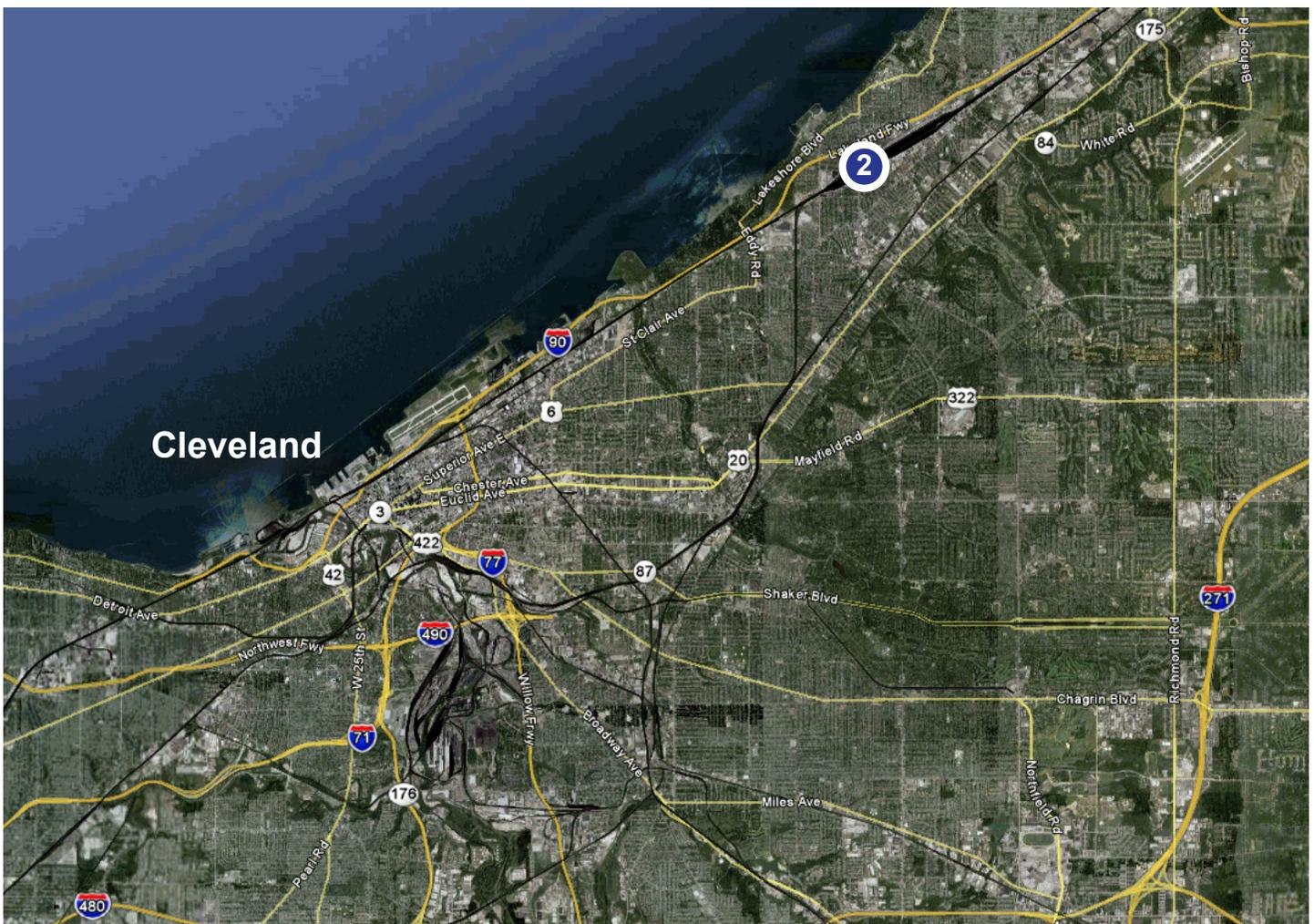
Location 601 East 152nd Street
Cleveland, OH 44110

Phone 216.268.7086
Fax 216.268.7071

Terminal Operations

Hours of Operation Mon–Fri: 7am–9pm
Sat: 7am–3pm
Closed Sunday

Terminal Capabilities COFC + TOFC



Directions

From the west, take I-90 E to Exit 180 B East 152nd Street. Turn right onto East 152nd Street. The terminal is on the left. From the east, take I-90 W to Exit 181 toward East 152nd Street. Turn left onto 152nd Street.

CSX Buckeye Yard Intermodal Terminal

Terminal Contact Information

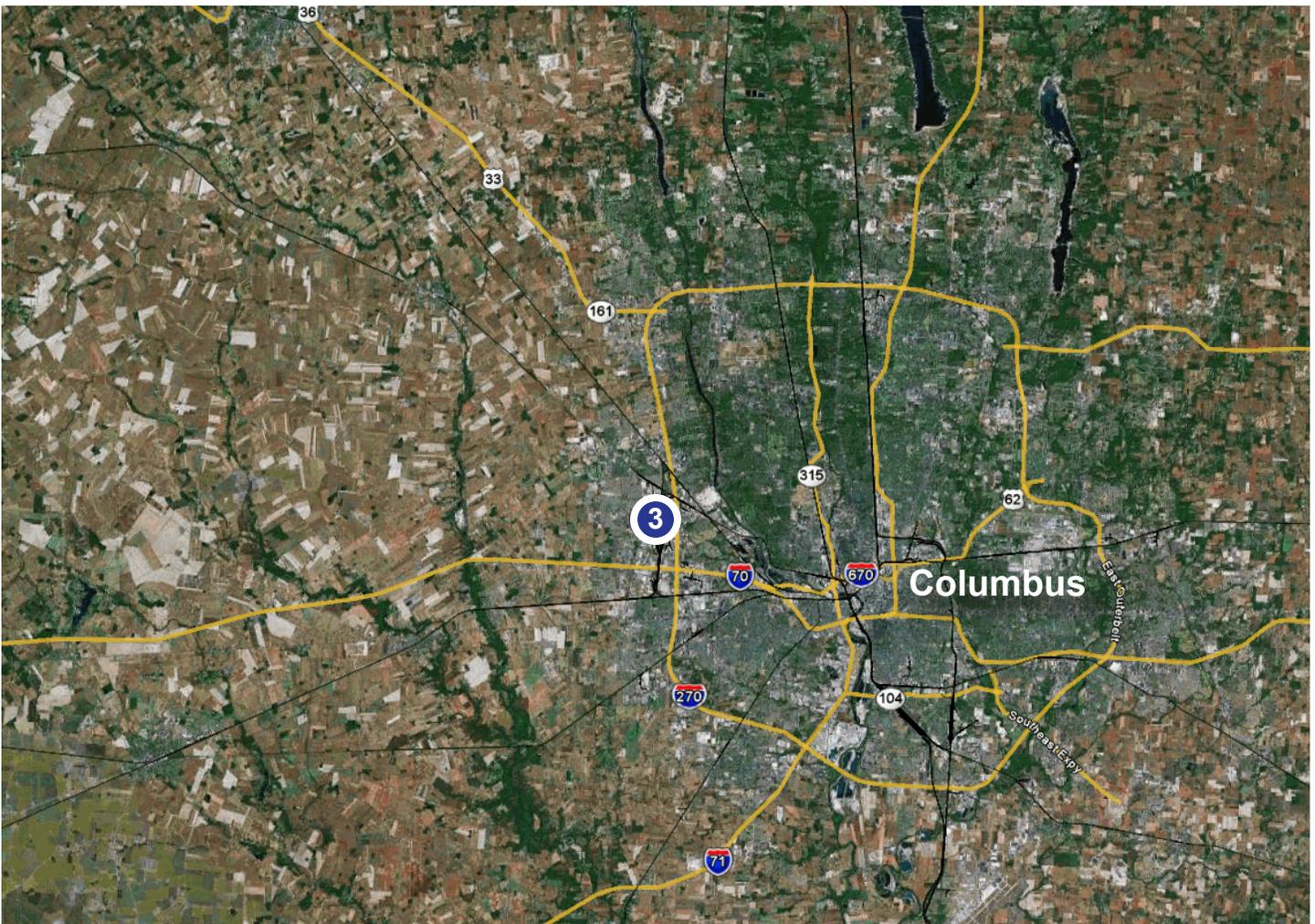
Location 2351 West Belt Drive
Columbus, OH 43228

Phone 614.850.1710
Fax 614.850.1719

Terminal Operations

Hours of Operation Mon–Fri: 24 Hours
Sat: 12am–4pm
Closed Sunday

Terminal Capabilities COFC + TOFC



Directions

From I-270 take Exit 10 Roberts Road. Turn west and follow Roberts Road. Turn left onto West Belt Drive.

CSX Queensgate Intermodal Terminal

CINCINNATI

Terminal Contact Information

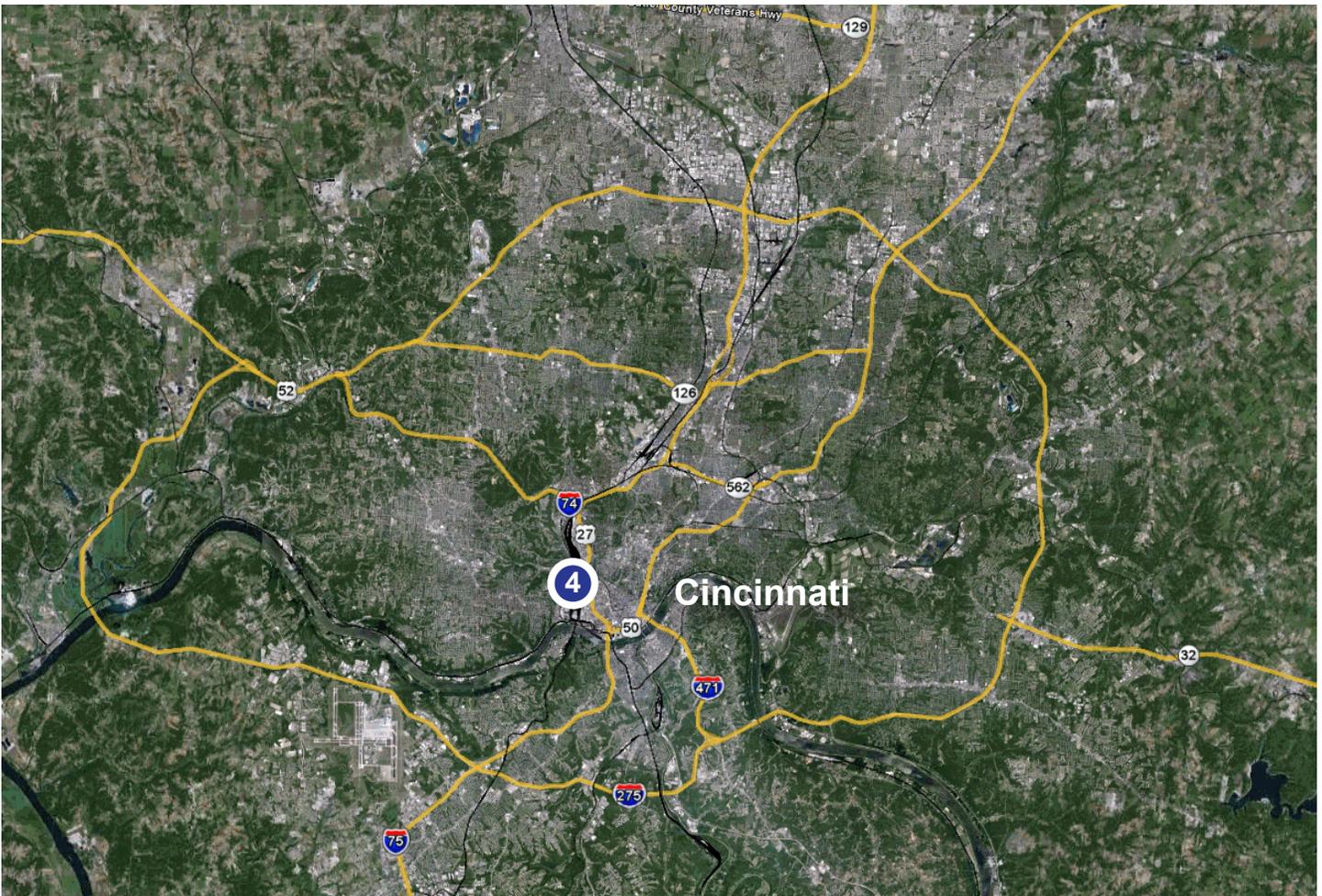
Location 2149 Western Avenue
Cincinnati, OH 45214

Phone 513.369.5490
Fax

Terminal Operations

Hours of Operation Mon–Fri: 24 Hours
Sat: 5:30am–6:30pm
Closed Sunday

Terminal Capabilities COFC + TOFC



Directions

Follow I-75 to signs for Ezzard Charles Street/Liberty Street and merge onto Winchell Avenue. Turn left onto Bank Street and turn right onto Western Avenue.

Marion Intermodal Center

MARION

Terminal Contact Information

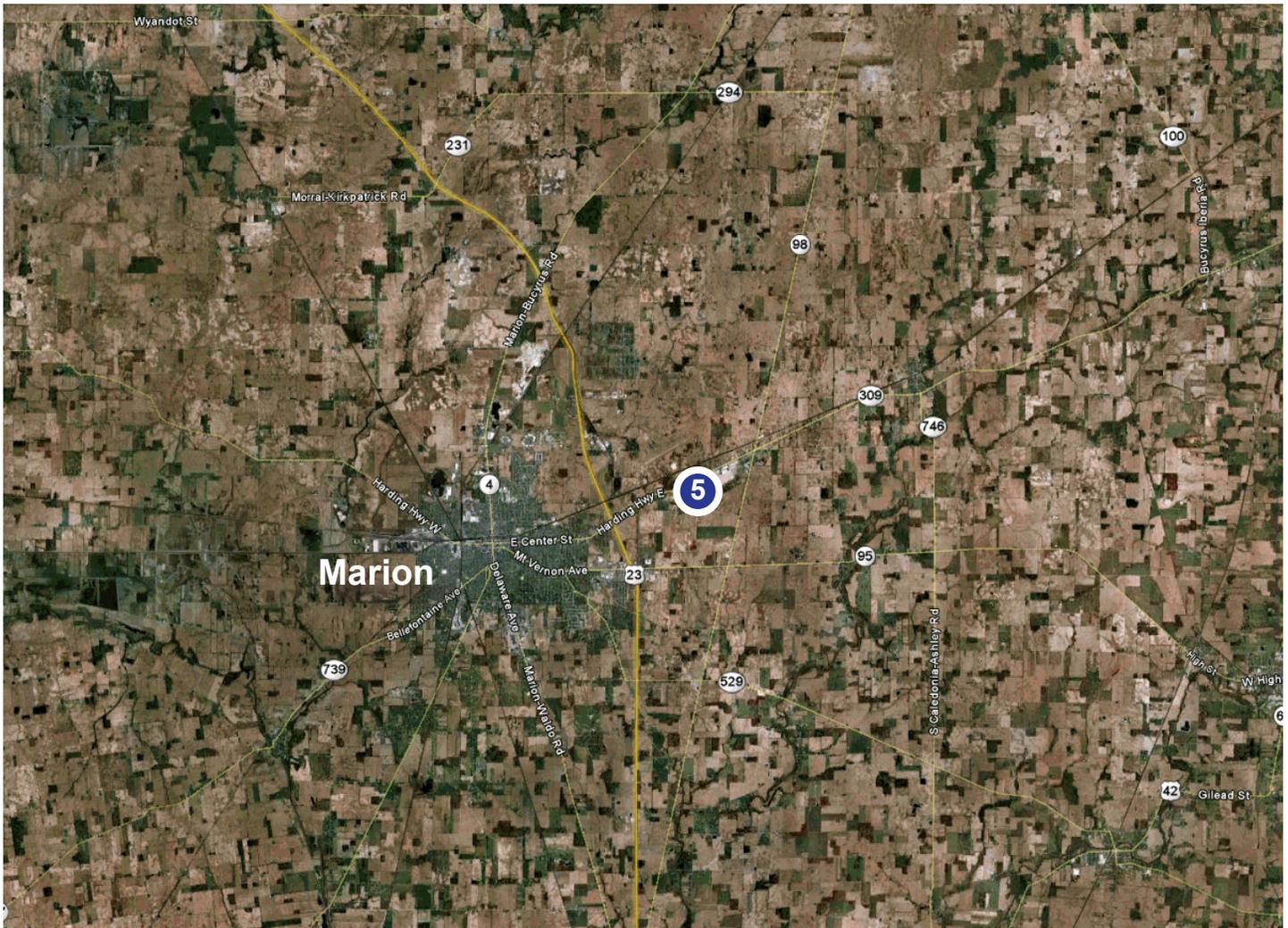
Location 3007 Harding Hwy East,
Marion, OH 43302

Phone 740.223.3505

Terminal Operations

Hours of Operation Mon–Fri: 7am –7pm
Some Saturday Service

Terminal Capabilities COFC + TOFC



Directions

From US 23 Exit to US 309 Harding Hwy East. Turn east and travel approximately 1 mile to the terminal. Turn right.

CSX Marysville Serving Honda of America

MARYSVILLE

Terminal Contact Information

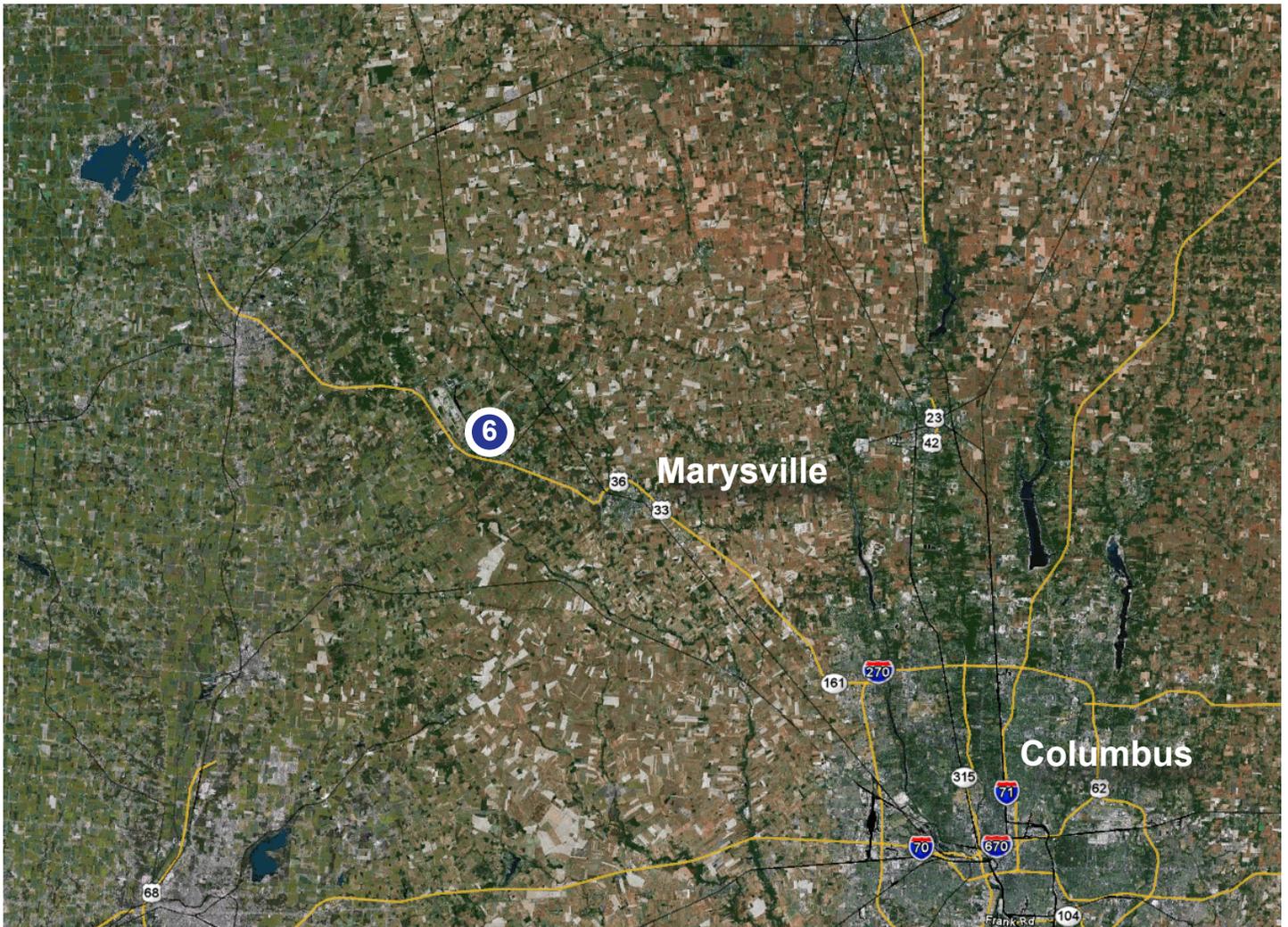
Location 20103 Johnson Road
Marysville, OH 43040

Phone 397.303.0000

Terminal Operations

Hours of Operation 7 days a week
24 hours a day

Terminal Capabilities COFC + Auto Ramp



Directions

From US 33, Exit Darby Pottersburg Road (Honda Parkway) and turn north. Follow Darby Pottersburg Road northeast and turn left onto Johnson Road.

Norfolk Southern Intermodal Services in Ohio

(Description provided by Norfolk Southern Corporation)

Norfolk Southern operates an extensive double-stack rail network, serving more major eastern US markets than any other rail carrier. In addition, NS offers connections to carriers in the western US and Canada through a variety of interchange locations. Trans-continental moves are interchanged at major gateways, including Chicago, Kansas City, Memphis, and Dallas. NS offers superior steel wheel interchange services with BNSF and/or UP at all of these locations, reducing potential delays in transit.

International shipments on Norfolk Southern include everything from raw materials such as waste paper, logs, and cotton, to finished consumer goods including furniture, electronics, auto parts, and clothing.

In addition to the Norfolk Southern land bridge services, NS also offers rail services from East Coast ports, which serve ships to and from Europe, the Far East and South America. On-dock rail is offered at the ports of Savannah, Norfolk, New York, and Jacksonville. On-dock facilities offer reduced drayage cost and more efficient transportation for intermodal customers. NS also serves near-dock facilities at Baltimore, Charleston, and Miami. Realizing the growing importance over the last several years of Atlantic port services, NS has worked closely with the East Coast ports to further improve the ship to rail transfer process and improve inland rail services.

Norfolk Southern's new Heartland Corridor officially opened to doublestack intermodal traffic in 2010. The Heartland Corridor is a PPP between NS and Virginia, West Virginia, Ohio and the federal government aimed at creating the shortest, fastest route for double-stack container trains moving between the Port of Virginia and the Midwest. As a result, NS is now able to route 100% of the traffic moving between Norfolk

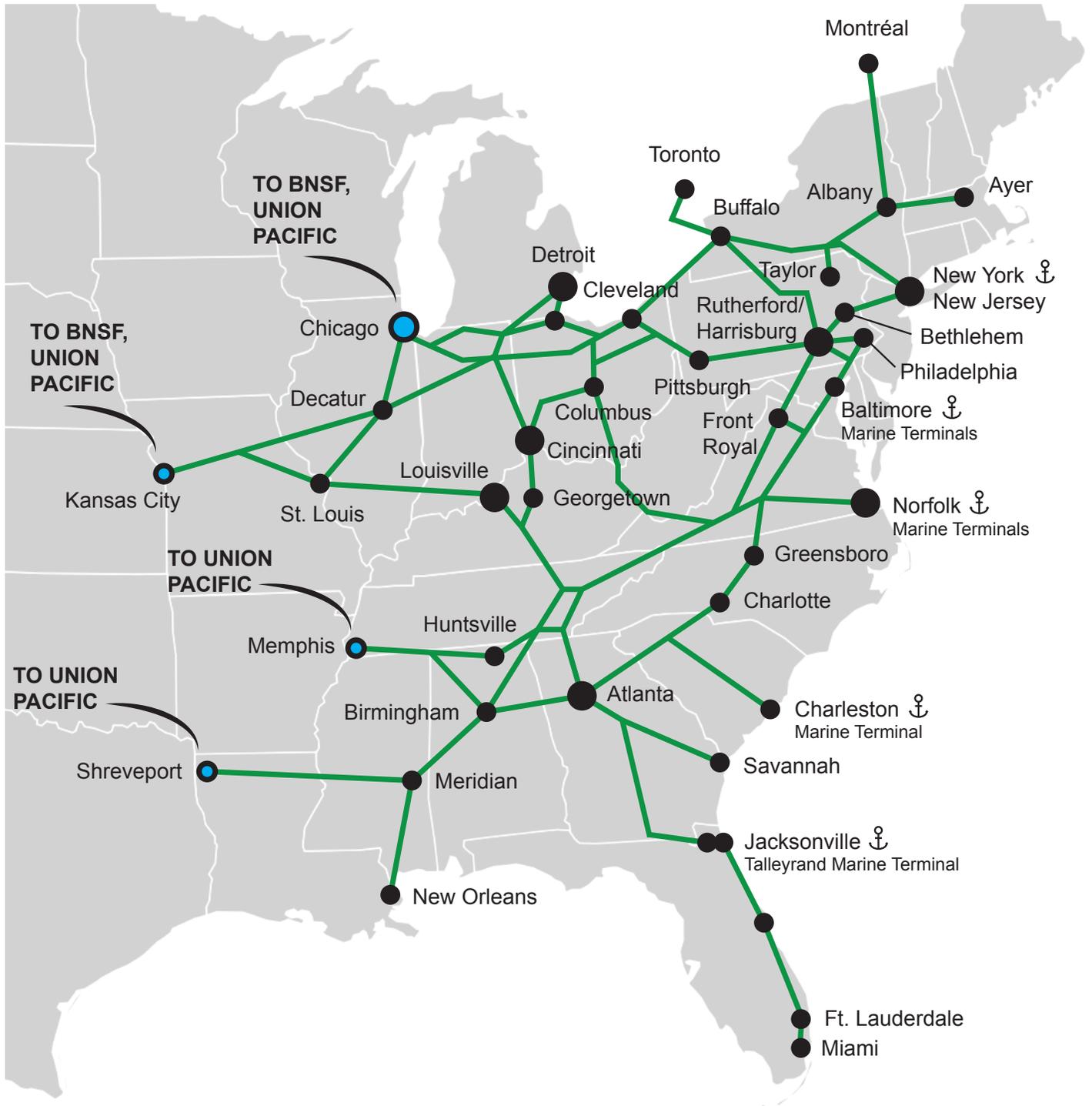
and Chicago over the corridor, significantly reducing transit times to just under 48 hours. New schedules are now available. The Heartland Corridor demonstrated the effectiveness of PPP to improve America's rail infrastructure.

Norfolk Southern's partners for domestic shipments include Intermodal Marketing Companies (IMCs) and carriers such as Truckload and Less-Than-Truckload (LTL) companies. These companies coordinate all aspects of the intermodal move while providing a single point of contact. NS operations and its terminal network create speed and reliability while offering customers an advantage. NS offers services between the West Coast and both the Northeast and the Southeast. NS also serves what is referred to as the "Golden Triangle" between Chicago, Illinois; Harrisburg, Pennsylvania; and Atlanta, Georgia.

The NS system of intermodal terminal facilities allow fast, efficient and consistent service throughout the twenty-two states directly served by NS east of the Mississippi River.

**Norfolk Southern's
new Heartland
Corridor officially
opened to double-
stack intermodal
traffic in 2010**

Norfolk Southern System Intermodal Network Map



RAIL MAP SYMBOLS

— NORFOLK SOUTHERN CORP.

- Terminal
- Multiple Terminals
- Interchange Facility
- ⚓ Marine Terminal

Norfolk Southern Rickenbacker Intermodal Terminal

COLUMBUS

Terminal Contact Information

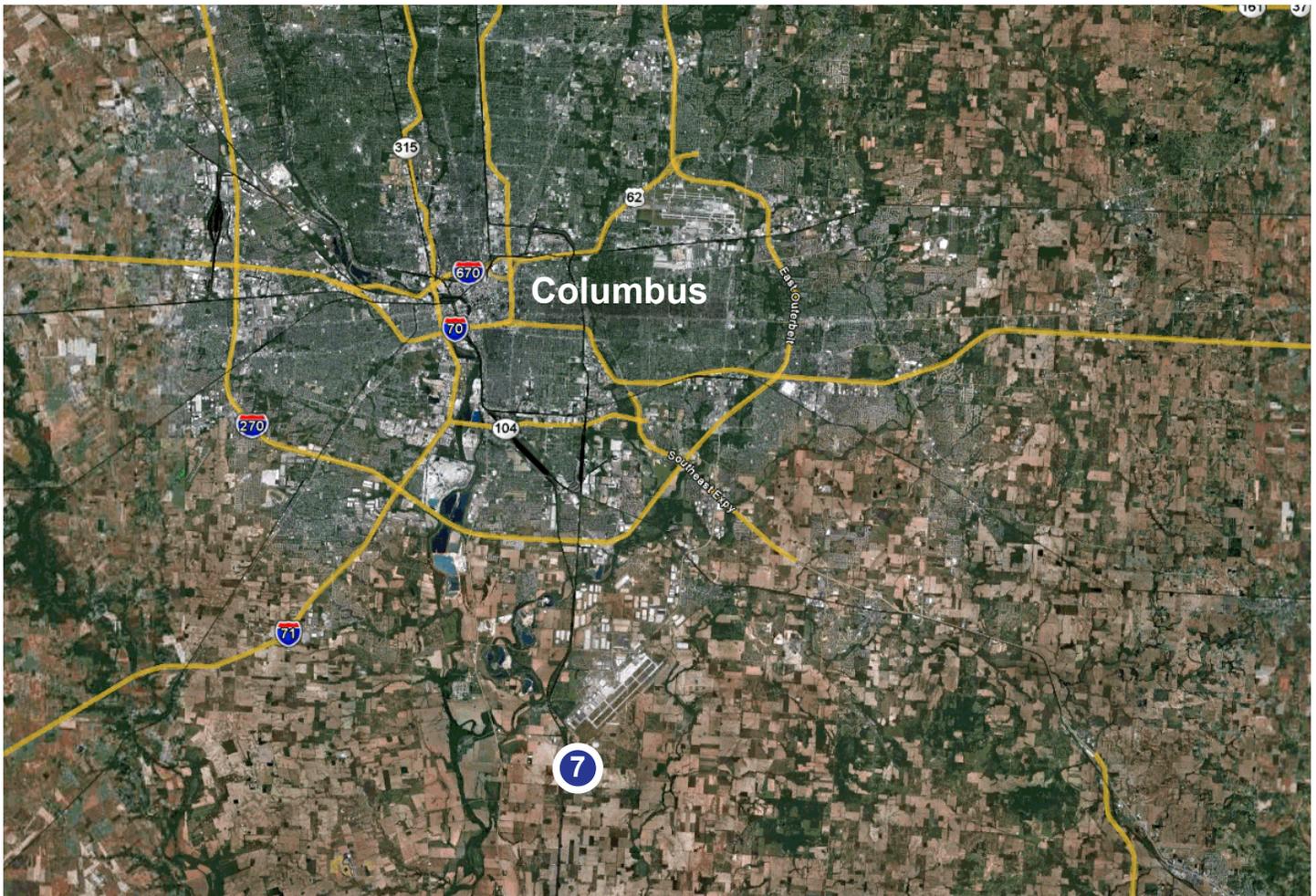
Location 3329 Thoroughbred Drive
Columbus, OH 43217

Phone Main Office: 614.492.4808
Customer Service: 800.497.2919

Terminal Operations

Hours of Operation 7 days a week
24 hours a day

Terminal Capabilities COFC + TOFC + Stack Car +
Bottom & Top Lift



Directions

Take I-270 to Exit 49 Alum Creek Dr. Turn South onto Alum Creek Dr. Turn Right onto Rickenbacker Parkway. Thoroughbred Road and the intermodal terminal are located at the intersection of Rickenbacker Parkway and Ashville Pike.

Norfolk Southern Maple Heights Intermodal Terminal

CLEVELAND

Terminal Contact Information

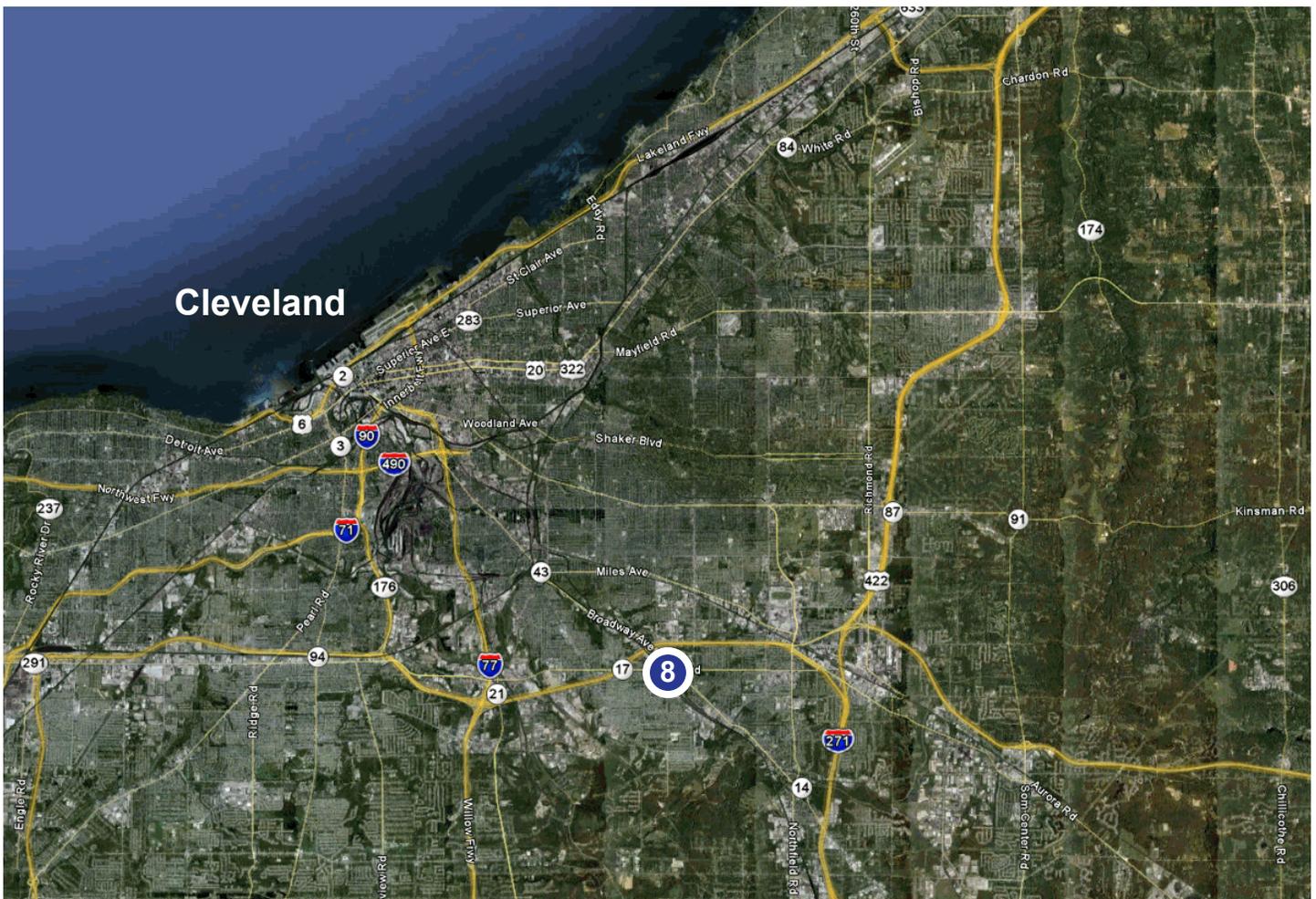
Location 5300 Greenhurst Drive
Maple Heights, OH 44137

Phone Customer Service: 800.497.2919

Terminal Operations

Hours of Operation Mon–Fri: 5am–9pm
Sat: 7am–3pm

Terminal Capabilities COFC + TOFC + Top Lift Only



Directions

From the west, take I-480 E to Exit 22 Granger Road/ OH 17. Continue to follow OH 17. Turn left onto Broadway Ave. Turn left onto Greenhurst Dr. From the east, take I-480 W to Exit 23 Broadway Ave/ OH 14. turn left onto Broadway Ave. Turn right onto Greenhurst Dr.

Norfolk Southern Gest Street Intermodal Terminal

Terminal Contact Information

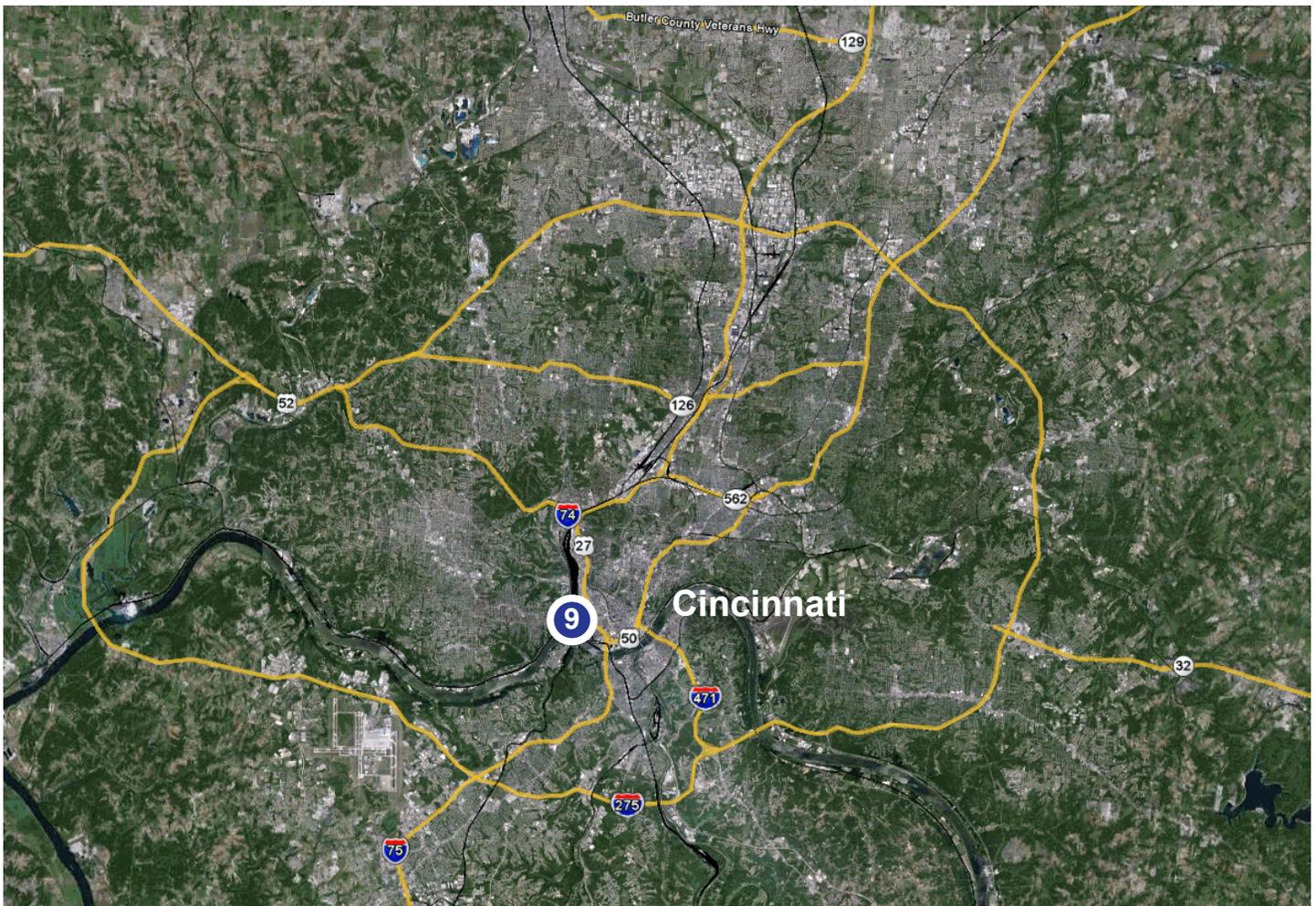
Location 1276 W. 8th Street
Cincinnati, OH 45203

Phone Customer Service: 800.497.2919
Fax: 513.977.3293

Terminal Operations

Hours of Operation Mon: 6am–Sat:10pm
Sun: 6am–10pm
Live Lift Hours

Terminal Capabilities COFC + TOFC + Bottom & Top Lift



Directions

From the South, take I-75 to Exit 1G Freeman Avenue. At the second light turn right onto West 8th. Drive two blocks to stop sign.

Norfolk Southern Sharonville Intermodal Terminal

Terminal Contact Information

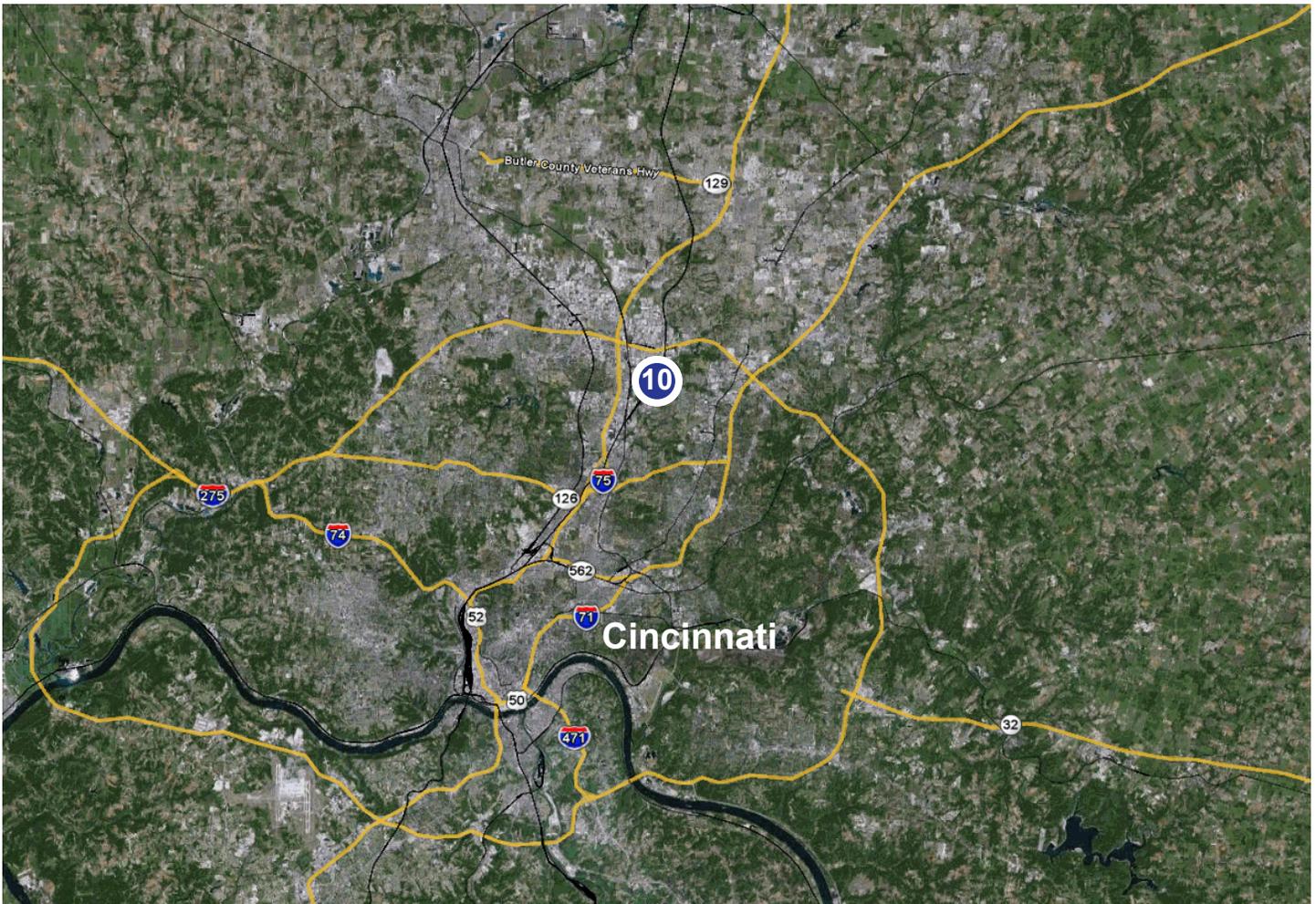
Location 3155 E. Sharon Road
Sharonville, OH 45241

Phone Customer Service: 800.497.2919
Fax: 513.772.2580

Terminal Operations

Hours of Operation Mon–Fri: 7am–5pm

Terminal Capabilities COFC



Directions

From the South, take I-75 N to Exit 15 Glendale/Sharon. At the end of the exit ramp, bear right onto E. Sharon Rd. Drive and continue one mile to the facility. From the North, take I-75 S to exit 15 (Glendale/Sharon). At the end of the exit ramp, turn left onto E. Sharon Rd. Drive one mile to facility.

Airline Junction Intermodal Terminal

TOLEDO

Terminal Contact Information

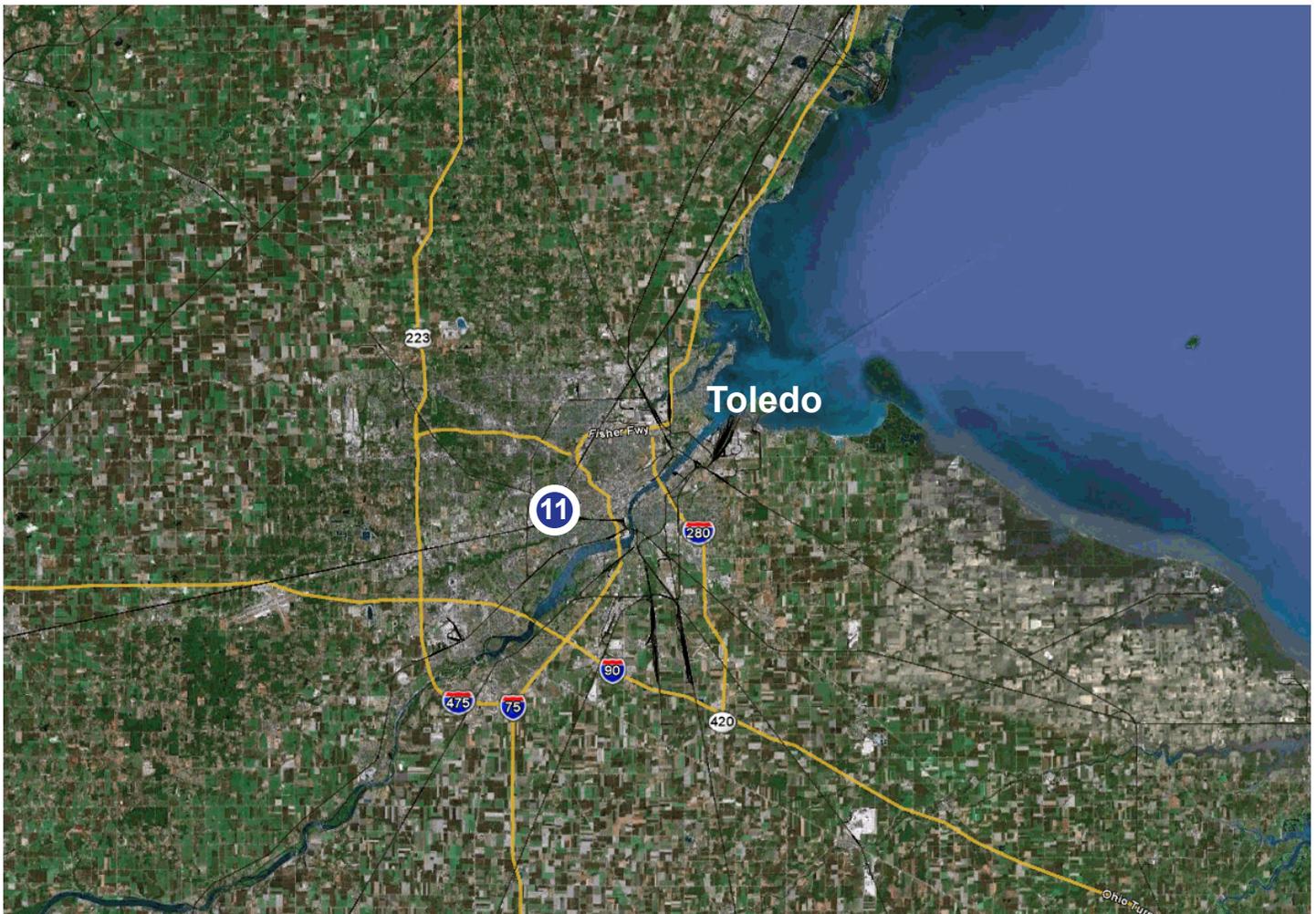
Location 2101 Hill Avenue
Toledo, OH 43607

Phone Main Office: 216.518.8403
Fax: 419.381.5572

Terminal Operations

Hours of Operation Mon–Fri: 7am–5pm

Terminal Capabilities COFC + TOFC



Directions

Take I-75 to Exit 201A (Collingwood Blvd.). If traveling I-75 South, turn right onto Collingwood. If traveling I-75 North, turn left onto Collingwood towards Ohio Route 25. Turn left onto Ohio Route 25. Turn right onto Ohio Route 2. Turn right onto Ohio Route 24. Turn left onto Hill Avenue.

Triple Crown RoadRailer® Network NS Subsidiary

SANDUSKY

Terminal Contact Information

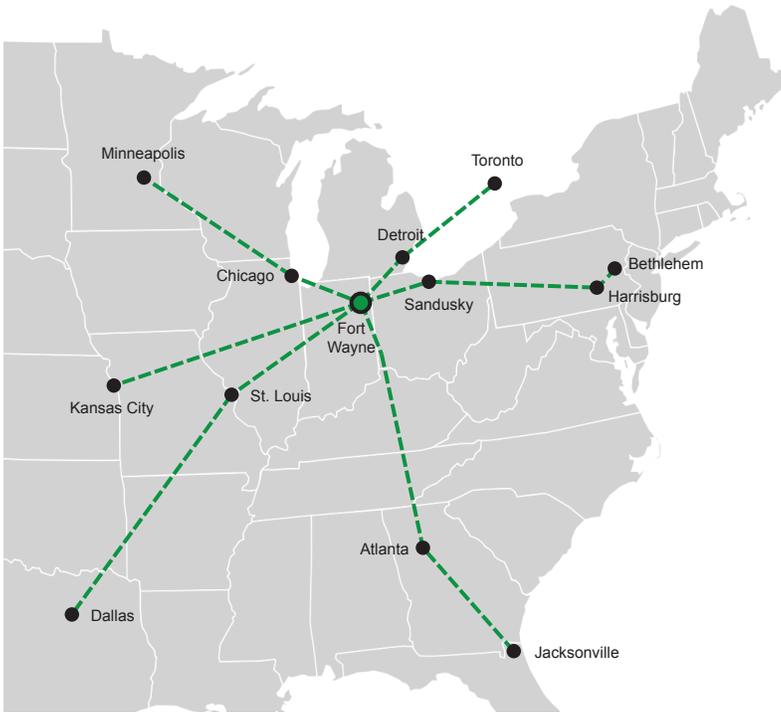
Location 3811 Old Railroad Road
Sandusky, OH 44870

Phone Triple Crown: 800.325.6510

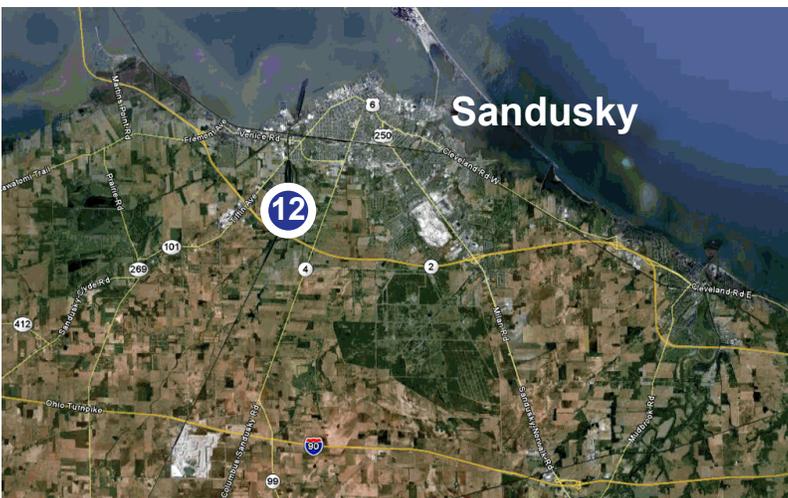
Terminal Operations

Hours of Operation 7 days a week
24 hours a day

Terminal Capabilities RoadRailer®



The Triple Crown service network is a classic hub-and-spoke design with Fort Wayne, Indiana as its hub. Truck trailers are picked up from customers and transported to the Triple Crown terminals where they are loaded onto specialized rail bogies or RoadRailer®. The RoadRailer® are then assembled into convoy units designated for specific destination terminals in the network.



Directions

From US 2 Exit OH 4 and turn north. Turn left onto W. Strub Road. Turn right on Old Railroad Road.

OHIO RAIL DEVELOPMENT COMMISSION



1980 West Broad Street
Columbus, Ohio 43223
614.644.0306
www.rail.ohio.gov